

2025



ADA SELF-EVALUATION AND TRANSITION PLAN



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For those who are deaf or hard of hearing, the Washington State Relay can be contacted at 711 for assistance in making a request to the City.

The Americans with Disabilities Act Notice:

In accordance with the requirements of Title II of the Americans with Disabilities Act of 1990 ("ADA"), the City of Cle Elum will not discriminate against qualified individuals with disabilities on the basis of disability in its services, programs, or activities.

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EXECUTIVE SUMMARY

The City of Cle Elum is committed to removing barriers to accessibility in its services, programs, and activities to prohibit discrimination based on disability. To achieve this end, the City has completed an Americans with Disabilities Act (ADA) Self-Evaluation and Transition Plan in accordance with federal guidance for the purpose of implementing capital projects committed to equal access for all, including those with disabilities. In developing this plan, the City of Cle Elum has undertaken a comprehensive evaluation of its facilities and policies related to the public rights-of-way to determine what types of access barriers exist for individuals with disabilities. This plan will be used to help guide future planning and implementation of necessary accessibility improvements. Removing all ADA noncompliant features and barriers is an undertaking that will take many years to achieve. This plan identifies priorities and recommendations that the City can implement over time to achieve an ADA-compliant public right-of-way.

What is an ADA Self-Evaluation and Transition Plan?

An ADA Self-Evaluation and Transition Plan includes four components:

- **Identify physical obstacles** in the public entity's facilities that limit the accessibility of its programs or activities to individuals with disabilities;
- **Describe in detail the methods** that will be used to make the facilities accessible;
- **Specify the schedule** for taking the steps necessary to achieve compliance with this section and, if the time period of the transition plan is longer than one (1) year, identify steps that will be taken during each year of the transition period; and
- **Indicate the official** responsible for implementation of the plan.

Both the Self-Evaluation and Transition Plan are required elements of the federally mandated ADA Title II, which requires that government agencies provide equal access to programs and services they offer. The City's initial phase was preparation of a Self-Evaluation focused on transportation elements within the public right-of-way. These major elements include bus stops, sidewalks, curb ramps, crosswalks, railroad crossings, miles of multi-use path, public-accessible parking, public-accessible building access, parks inventory, and pedestrian control devices (pushbuttons) at intersections. This evaluation included a comparison of each facility to current ADA standards. This evaluation did not include an assessment of whether the facility was constructed to meet current ADA standards to the maximum extent feasible as allowed by the ADA. As such, the Self-evaluation and resulting costs and schedule for full ADA transition are considered conservative. The Self-Evaluation also includes an assessment of practices, policies, and procedures that relate to the planning, design, and construction of these facilities.

This Plan summarizes the Self-Evaluation phase which includes an accessibility assessment of pedestrian facilities as well as practices and procedures. The Transition Plan identifies an implementation schedule for the removal of non-compliant barriers, outlines a grievance procedure, and a request for information procedure for the City's residents.



CHAPTER 1 – INTRODUCTION

The Americans with Disabilities Act (ADA) was enacted on July 26, 1990, and provides comprehensive civil rights protections to persons with disabilities in the areas of employment, state and local government services, and access to public accommodations, transportation, and telecommunications.

1.1 PLAN REQUIREMENT

The Americans with Disabilities Act (ADA), enacted on July 26, 1990, is a civil rights law prohibiting discrimination against individuals on the basis of disability. ADA consists of five titles outlining protections in the following areas:

1. Employment
2. State and local government services
3. Public accommodations
4. Telecommunications
5. Miscellaneous Provisions

Title II of the ADA requires public entities, such as Cle Elum, to make their existing “programs” accessible “except where to do so would result in a fundamental alteration in the nature of the program or an undue financial and administrative burden.” Public right-of-way, public government buildings, and public parks all fall within the city’s programs.

As required by [ADA Title II, § 35.105](#), Cle Elum has conducted a self-evaluation of its facilities within public rights of way and has developed this Transition Plan detailing how the organization will ensure that city facilities are accessible to all individuals.

1.2 ADA AND ITS RELATIONSHIP TO OTHER LAWS

Title II of ADA is companion legislation to two previous federal statutes and regulations: the [Architectural Barriers Acts of 1968](#) and [Section 504 of the Rehabilitation Act of 1973](#).

The Architectural Barriers Act of 1968 is a Federal law that requires facilities designed, built, altered or leased with Federal funds to be accessible. The Architectural Barriers Act marks one of the first efforts to ensure access to the built environment.

Section 504 of the Rehabilitation Act of 1973 is a Federal law that protects qualified individuals from discrimination based on their disability. The nondiscrimination requirements of the law apply to employers and organizations that receive financial assistance from any Federal department or agency. Title II of ADA extended this coverage to all state and local government entities, regardless of whether they receive federal funding or not.



1.3 AGENCY REQUIREMENTS

Under Title II, Cle Elum must meet these general requirements:

- Must operate their programs so that, when viewed in their entirety, the programs are accessible to and useable by individuals with disabilities ([28 C.F.R. Sec. 35.150](#)).
- May not refuse to allow a person with a disability to participate in a service, program or activity simply because the person has a disability ([28 C.F.R. Sec. 35.130 \(a\)](#)).
- Must make reasonable modifications in policies, practices and procedures that deny equal access to individuals with disabilities unless a fundamental alteration in the program would result ([28 C.F.R. Sec. 35.130\(b\) \(7\)](#)).
- May not provide services or benefits to individuals with disabilities through programs that are separate or different unless the separate or different measures are necessary to ensure that benefits and services are equally effective ([28 C.F.R. Sec. 35.130\(b\)\(iv\) & \(d\)](#)).
- Must take appropriate steps to ensure that communications with applicants, participants and members of the public with disabilities are as effective as communications with others ([28 C.F.R. Sec. 35.160\(a\)](#)).
- Must designate at least one responsible employee to coordinate ADA compliance. This person is often referred to as the "ADA Coordinator." The public entity must provide the ADA coordinator's name, office address, and telephone number to all interested individuals ([28 CFR Sec. 35.107\(a\)](#)).
- Must provide notice of ADA requirements. All public entities, regardless of size, must provide information about the rights and protections of Title II to applicants, participants, beneficiaries, employees, and other interested persons ([28 CFR Sec. 35.106](#)). The notice must include the identification of the employee serving as the ADA coordinator and must provide this information on an ongoing basis.
- Must establish a grievance procedure. Public entities must adopt and publish grievance procedures providing for prompt and equitable resolution of complaints ([28 CFR Sec. 35.107\(b\)](#)). This requirement provides for a timely resolution of all problems or conflicts related to ADA compliance before they escalate to litigation and/or the federal complaint process.

This document has been created to specifically cover accessibility within the public rights of way and does not include information on Cle Elum programs, practices, or building facilities not related to public rights of way.



CHAPTER 2 – SELF-EVALUATION

Title II of the Americans with Disabilities Act (ADA) requires that jurisdictions evaluate services, programs, policies and practices to determine whether they comply with the non-discrimination requirements of the ADA.

This chapter describes the methods and findings of the Self-Evaluation. Section 2.1 provides an overview of ADA-related City policies. Next, Section 2.2 reviews City practices and design standards. Finally, Section 2.3 summarizes the Self-Evaluation's field data collection methods and findings regarding existing pedestrian facilities, such as sidewalks and curb ramps.

2.1 POLICY REVIEW

The City of Cle Elum is required, under Title II of the Americans with Disabilities Act (ADA) and [28 C.F.R. Sec. 35.105](#), to perform a self-evaluation of its current transportation infrastructure policies, practices, and programs. This self-evaluation will identify what policies and practices impact accessibility and examine how the city implements these policies. The goal of the self-evaluation is to verify that, in implementing the city's policies and practices, the department is providing accessibility and not adversely affecting the full participation of individuals with disabilities.

The self-evaluation also examines the condition of the city's existing public facilities and identifies potential need for infrastructure improvements. This will include the sidewalks, curb ramps, bicycle/pedestrian trails, bus stops and traffic control signals that are located within the city rights of way. Any barriers to accessibility identified in the self-evaluation and the remedy to the identified barrier are set out in this transition plan.

2.1.1 Administrative requirements

Title II requires that public entities undertake five administrative steps to promote implementation of the ADA:

2.1.1.1 Self-Evaluation

This Self-Evaluation is the first step in implementing ADA requirements throughout the City of Cle Elum.

2.1.1.2 Public Notice

The City of Cle Elum is required to post public notice that it does not discriminate on the basis of disability provided in print and electronic formats. While there are some official city documents that include public notice about ADA accommodation, the city should consider implementing a standard ADA Notice for the website and official public documents.



Below is a draft example:

The City of Cle Elum does not discriminate on the basis of disability in its programs, services, activities and employment practices.

If you need auxiliary aids and services for effective communication (such as a sign language interpreter, an assistive listening device or print material in digital format) or reasonable modification to programs, services or activities contact the ADA Coordinator as soon as possible, preferably 5-business days before the activity or event.

A grievance procedure is available to resolve complaints.

Upon request, this notice is available in alternative formats such as large print or Braille.

Rob Omans, City Administrator. romans@cleelum.gov. 509-674-2262

2.1.1.3 Grievance Procedure

On June 14, 2021, the City of Cle Elum adopted Resolution No. 2021-011, which memorialized acceptance to the Community Development Block Grant (CDBG) loan application grievance procedure.

The City of Cle Elum is in the process of updating its Grievance Procedure for compliance with the ADA – see Appendix C.

2.1.1.4 ADA Coordinator

The Designated ADA Coordinator is City Administrator, Rob Omans. romans@cleelum.gov. 509-674-2262.

2.1.1.5 Develop Transition Plan

The development of a Transition Plan is a key component of this project and will address barriers in facilities that affect equal participation of people with disabilities in the City of Cle Elum's programs, activities, and services.

2.1.2 Effective Communication

Many people who are blind or have low vision, who are deaf or hard of hearing, or who have other communication disabilities are prevented from participating fully in programs, activities, and services because of communication issues. To address this, Title II requires that communication with people with disabilities be as effective as communication with others. Oftentimes, good communication practices will suffice, such as not turning away when speaking with a person who is hard of hearing or taking the time to listen to a person who has a speech impairment. Other times a public entity needs to provide "auxiliary aids and services" to assure effective communication.



The City Council passed Resolution No. 2022-023 which provides access to telephone and communication services, but the page on the website could be challenging to find and has a lengthy url: <https://cleelum.gov/phone-service-access-for-people-who-are-deaf-deaf-blind-hard-of-hearing-and-speech-disabled/>

2.1.3 Comprehensive Plan

The Comprehensive Plan is the long-range planning document for the city. The latest version of this plan was completed for the years 2017-2037. It is currently (2026) being updated with a 2046 time horizon, however, the update will not be complete until the end of 2026. The plan covers topics including land use, transportation, economic development, housing, capital facilities and utilities.

The Comprehensive Plan Transportation (T), Parks and Recreation (PR), elements include the following goals and policies related to ADA facilities:

Transportation Element

- Goal T-1: Be consistent with the City's Comprehensive Plan Goals and Policies, the State's Growth Management Act, and County-wide Planning Policies.
- Policy T-4: Adequate transportation facilities and services should be in place at the time of occupancy of a development.
- Policy T-7: Ultimately, land use patterns should support transit and non-motorized modes of travel.
- Goal T-2: Create a comprehensive street system that provides reasonable vehicular circulation throughout the City while enhancing the safety and function of the overall local transportation.
- Policy T-10: Streets and pedestrian paths in residential neighborhoods should be arranged as an interconnecting network that serves local traffic and facilitates pedestrian circulation.
- Goal T-4: Promote the development and enhancement of non-motorized transportation Citywide.
- Policy T-25: Pedestrian and bicycle traffic should be accommodated within all areas of the City.
- Policy T-26: Pedestrian and bicycle movement across arterial intersections should be enhanced.
- Policy T-27: Obstructions and conflicts that restrict pedestrians and bicycle movement should be minimized on sidewalks, paths and other areas.
- Policy T-28: Bicycle parking and storage facilities should be encouraged within development projects, in commercial areas, and in parks.
- Policy T-29: Streets and pedestrian paths in residential neighborhoods should be arranged as interconnecting networks and should connect to other streets.
- Policy T-30: New pedestrian facilities should be compliant with the Americans with Disabilities Act, and existing facilities should be upgraded to improve accessibility.
- Policy T-31: Non-motorized transportation should be developed in tandem with motorized transportation systems, recognizing issues such as safety, user diversity, and experiential diversity.



- Policy T-32: Recognize the diversity of transportation modes and trip purposes for the following three groups: pedestrians, bicyclists, other non-motorized wheels.
- Policy T-33: Foot/bicycle separation should be provided whenever possible; however, where conflict occurs, foot traffic should be given preference.
- Policy T-34: Adequate separation between non-motorized traffic should be provided to ensure safety.
- Policy T-35: Appropriate mitigation measures should be taken to address the impacts to the City's transportation infrastructure. Contributions
- Policy T-36: Encourage security, maintenance, and cleanliness of pedestrian facilities.
- Policy T-39: Encourage the principles of pedestrian design, whenever possible, on new and existing pedestrian facilities.
- Policy T-40: Encourage the removal and/or maintenance of vegetation that impedes sight lines or the travel surface of pedestrian and bicycle facilities.
- Policy T-41: Whenever practicable require that storm drains, utility boxes and other similar infrastructure on or near road shoulders be located outside of these travel ways. When not practicable, these improvements shall be flush with the travel surface to create a viable pedestrian/bicycle travel lane.
- Policy T-42: Cle Elum seeks to enable, whenever possible, residents to travel more safely and efficiently throughout the City on foot, by bicycle, and by wheelchair.

Parks and Recreation Element

- Goal PR-1: Develop an outstanding parks, recreation and open space system in Cle Elum to meet the needs of a diverse community.
- Policy PR-2: Locate facilities and resources where they are accessible to the public.
- Policy PR-8: Where appropriate and practicable provide parks, recreation, and open space facilities that are accessible to all.
- Goal PR-2: Acquire and develop a City-wide, integrated, multiple-use track, trail, and connection system that is functional, safe, and convenient.
- Policy PR-13: Provide for the safe integration of bicycle, pedestrian, equestrian, and motorized multi-use networks where appropriate.
- Policy PR-14: Encourage the development of pedestrian rights-of-way and well lighted trails which can provide safe passage between neighborhoods, schools, commercial, and recreational areas.

2.2 PRACTICES AND DESIGN STANDARDS

This section summarizes a review of the City of Cle Elum Design and Construction Standard Details and the Municipal Code to identify any barriers to accessible design.

The City of Cle Elum primarily addresses pedestrian facilities in their Municipal Code and the Standard Details within the City Design and Construction Standards. There are also policies related to pedestrian facilities in the City Comprehensive Plan.

The policies and standards were reviewed against the Access Board's Proposed Guidelines for Pedestrian Facilities in the Public Right-of-Way, PROWAG 2023 and recommendations were provided to fill gaps as they relate to the ADA.



Practices and design standards that meet accessibility standards are essential to ensure that new or upgraded pedestrian facilities are accessible and therefore reduce the number of accessibility barriers throughout the city.

2.2.1 Method

The Cle Elum Standard Details and Municipal Code were reviewed for compliance with ADA guidelines found in the [2023 PROWAG](#).

2.3 EXISTING FACILITIES

The Self-Evaluation inventoried barriers to access as required by ADA Title II Part 35, Subpart D – Program Accessibility § 35.150 (d)(3). Each facility and associated barriers were field inventoried and cataloged. Field data was collected by HLA Engineering and Land Surveying, Inc. (HLA) in 2024.

- Curb Ramps
- Signal Pushbuttons
- Accessible Parking
- Bus Stops
- Crosswalks
- Sidewalks
- Bike/Multi Use Path
- Public Buildings
- Park Accessibility
- Railroad Crossing

2.3.1 Public ROW

Many existing pedestrian features within City of Cle Elum right-of-way contain barriers and require improvements to meet current ADA standards. It is important to note that many of these facilities were constructed before the adoption of current ADA standards, and likely met applicable state and federal standards at the time of construction. Additionally, it is important to note that ADA regulations require facilities to be made accessible to the “maximum extent feasible,” (MEF) in “circumstances when the unique characteristics of terrain prevent the incorporation of accessibility features” (U.S. Department of Justice, 28 CFR § 35.151 New construction and alterations). These circumstances are often a result of adjacent topography or otherwise constrained locations, which are common to the Cle Elum road system. This plan’s Self-evaluation examined whether facilities were compliant with current ADA design requirements; it did not examine whether noncompliant facilities were built to the maximum extent feasible or practical.

2.3.1.1 Method

The Self-Evaluation identified the following:

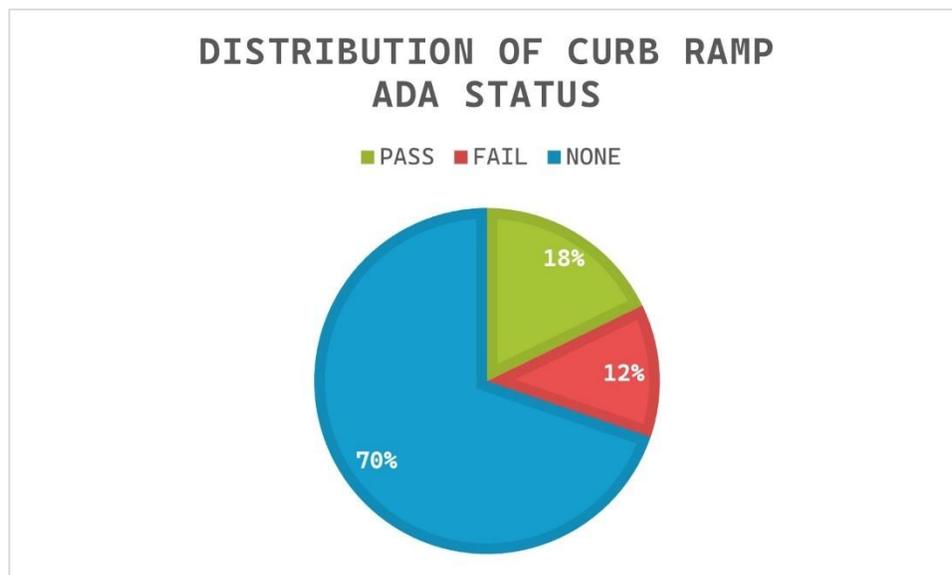


- The physical inventory of pedestrian facilities, as shown in the Inventory Maps in Appendix A, included:
 - 35.77 miles of sidewalk
 - 693 curb ramps
 - 38 signal pushbuttons
 - 364 crosswalks
 - 13 bus stops
 - 7 park facilities
 - 0.79 miles (4153.1 ft) of multi-use path
 - 4 public buildings
 - 10 public parking stalls
 - 2 railroad crossings

Additional detail regarding the Self-Evaluation’s findings for curb ramps, sidewalks, and pedestrian pushbuttons is provided in the following sections.

Curb Ramps

Curb ramps were assessed based on their type, but all followed broad categories including width, slope, clearance, tactile warning surface, and direction of travel. For a curb ramp to pass, it must pass all the listed criteria. The curb ramps in the city of Cle Elum were split between pass and fail. Most failed due to slope of the curb ramp or the absence of a tactile warning strip/damaged tactile warning strip.



- **Discontinuities:** Level changes of any kind are not allowed in curb ramps (PROWAG R304.5.4).
- **Tactile Warning Surface:** All curb ramps must have a tactile warning surface with a width of 4 feet minimum, or the full width of the ramp, which is located at



the bottom of the ramp and the back of the curb. Warning surface shall follow PROWAG R305 which states dome size, dome spacing, and contrast of light-on-dark or dark-on-light (PROWAG R305, 2010 ADA Design Standards).

- **Direction of Travel:** The ramp must be aimed in the direction of the adjacent crosswalk without crossing into roadway and shall be wholly contained within the markings at marked crosswalks (PROWAG R304.5.3, ADA 406, WSDOT ADA Field Guide). Additionally, one curb ramp shall be provided for each crosswalk at an intersection unless physical constraints make compliance infeasible (PROWAG R203.6).
- **Clearance:** Clearance must be provided in the form of a pedestrian access route which connects to the curb ramp (PROWAG R302).
- **Width:** All types of curb ramps must have a width of 48 inches minimum (PROWAG R304.5.1).
- **Perpendicular Curb Ramps:** Perpendicular curb ramps must not exceed a running slope of 8.3% and a cross slope of 2.0%. Flares must not exceed a 10% slope running parallel to the curb. A landing of 48 by 48 inches shall be provided at the top of the ramp with a running slope and cross slope no greater than 2.0% (PROWAG R304.2, WSDOT ADA Field Guide).
- **Parallel Curb Ramps:** Parallel curb ramps shall have a running slope no greater than 8.3% and a cross slope no greater than 2.0%. Landings shall be between the two sides of the ramp and not exceed a running or cross slope of 2.0% (PROWAG R304.3, WSDOT ADA Field Guide).

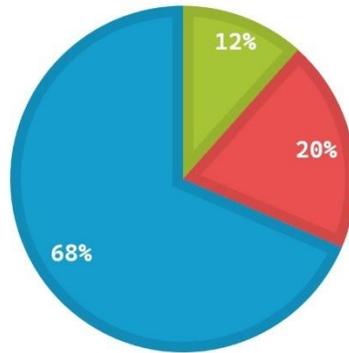
Sidewalks

Sidewalks were assessed by four main categories including width, slope, heaving, and obstructions. For a section of sidewalk to pass, it must be compliant with all criteria listed within these categories. Where sidewalk was available to be evaluated in the city of Cle Elum, 2/3 of it failed mainly due to slope, heaving, or obstruction. About 1/3 of the sidewalk evaluated passed ADA standards. A large majority of the inventory is made up of areas with no sidewalk present along with a small portion currently under construction.



DISTRIBUTION OF SIDEWALK ADA STATUS

■ PASS ■ FAIL ■ NONE



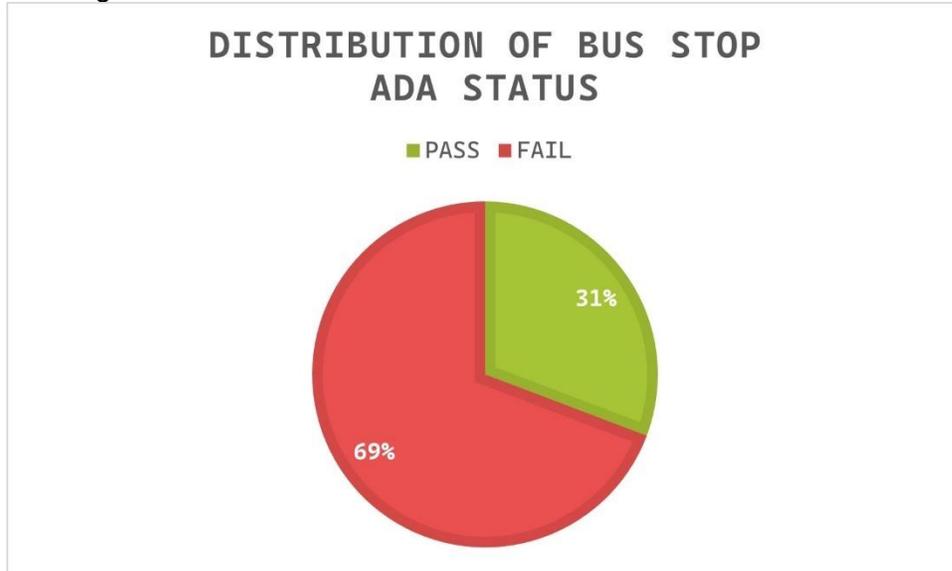
- **Width:** Sidewalks shall maintain a continuous width of 48 inches minimum. Where the path is less than 60 inches, there must be a passing space provided every 200 feet which is 60 inches by 60 inches minimum (PROWAG R302).
- **Slope:** The cross slope of any section of sidewalk shall not be steeper than 1:48 (2.0%) and the running slope shall not exceed the general grade of the adjacent street (PROWAG R302, 2010 ADA Design Standards, 403.3 & WSDOT ADA Field Guide).
- **Heaving:** Heaving shall not exceed $\frac{1}{4}$ inch vertically except in the case of an additional $\frac{1}{4}$ inch that is beveled at a slope no greater than 1:2 (50%) (PROWAG R302, 2010 ADA Design Standards 302 & WSDOT ADA Field Guide). This includes holes, grates and other coverings as well with the long dimension of grate openings running perpendicular to the direction of travel.
- **Obstruction:** Obstructions in the walking path include signs, poles, or other obstacles one might encounter. Non-pole-mounted objects with leading edges greater than 27 inches and less than 80 inches shall not protrude greater than 4 inches horizontally. This also implies that a vertical clear space of 80 inches minimum is required to pass under an object. Post-mounted objects shall overhang 4 inches maximum on either side when located between 27 inches and 80 inches from the ground. Obstructions shall not reduce the required 48-inch width of any pedestrian access route (PROWAG R402, WSDOT ADA Field Guide).

Bus Stops

Bus stops were assessed by two main categories including an accessible route and boarding areas. For a bus stop to pass, it must have passed all the criteria listed within these categories. Three of the thirteen bus stops were up to standard. Nine bus stops in the city of Cle Elum failed



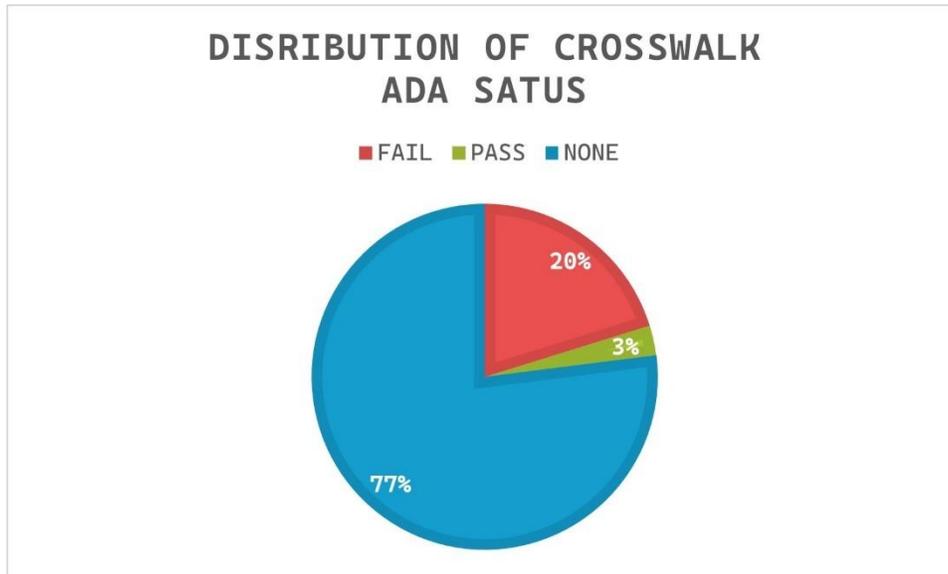
due to the absence of a boarding area which met ADA standards. One bus stop was under construction during the time of the evaluation.



- **Accessible Route:** If bus stops included a bus shelter, an accessible route had to be provided. The running slope of an accessible route shall not be steeper than 1:20 (5.0%) and a cross slope shall not be steeper than 1:48 (2.0%) (PROWAG R302, 2010 ADA Design Standards 402 & 403).
- **Boarding Area:** Boarding areas shall have a firm stable surface that is at least 96 inches long and 60 inches wide and shall not exceed the general slope of the roadway going parallel to it, while perpendicular to the roadway shall not exceed the 2.0% slope (PROWAG R309.1, 2010 ADA Design Standards 810.2-810.4).

Crosswalks

The crosswalks in the city of Cle Elum were assessed on four main categories including the condition of the paint or thermoplastic, the width and length of the individual bars in the crosswalk, the slope of the crosswalk, and where the ADA curb ramp is in conjunction with the crosswalk. For crosswalks to pass, they must meet all requirements listed within these categories. All the crosswalks in the city of Cle Elum failed due to majorly faded paint or chipping in thermoplastic. Since the paint failed first, nothing further was evaluated.



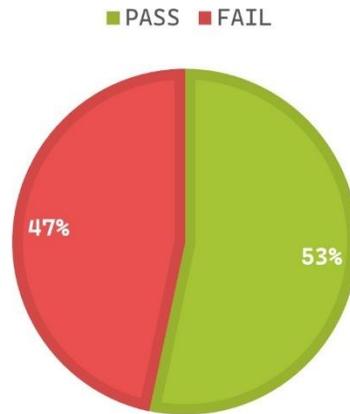
- **Paint or Thermoplastic Condition:** When checking the condition of the paint or thermoplastic, we looked for any fading or cracking visible. If the bars of the crosswalk paint or thermoplastic were faded, chipped, or cracked, the crosswalk failed (WSDOT Standard Plan M-15.10-02)
- **Width and Length of Bars:** Crosswalk length and width of bars were assessed by a 1-2 feet width and a minimum length of 4 feet (WSDOT Standard Plan M-15.10-02).
- **Slope:** Where a crosswalk is located at a stop control device, yield sign, or traffic control signal, the cross slope shall be no greater than 2.1% and the running slope shall be no greater than 5.0% unless the roadway design requires a steeper slope. Where a crosswalk is located at an uncontrolled approach, the cross slope shall be no greater than 5.0% and the running slope shall be no greater than 5.0% unless the roadway design requires a steeper slope at the location of the crosswalk (PROWAG R302)
- **Conjunction with ADA Curb Ramp:** When observing the crosswalk in conjunction with its adjacent ADA curb ramp, the curb ramp must be contained within the markings of the crosswalk (PROWAG 304.5.3, WSDOT ADA Field Guide).

Multi-use Pathway

The multi-use pathway in the city of Cle Elum was assessed on related categories as sidewalk including width, slope, heaving, and obstruction. For a section of multi-use pathway to pass, it must pass all criteria listed within these categories. The only multi-use pathway evaluated in the city of Cle Elum was split between pass and fail. The sections that failed were mostly due to discontinuities in heaving and cracking, and major cross slope.



DISTRIBUTION OF BIKE & MULTI-USE PATHWAY ADA STATUS



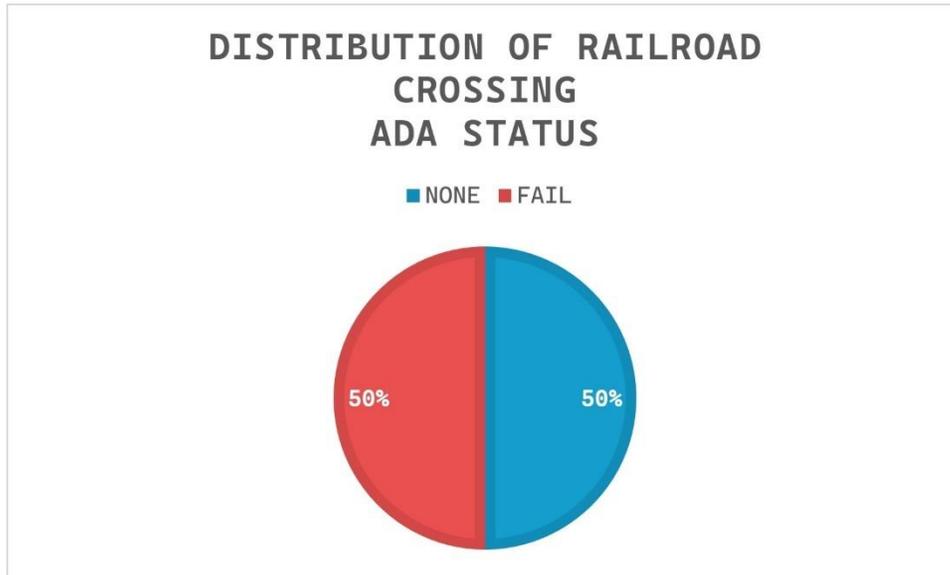
- **Width:** The width of any section of the multi-use pathway shall be a minimum of 48 inches and extend the full width of the path. Passing space shall be provided every 200 feet where the path is less than 60 inches. Passing space shall be 60 inches by 60 inches minimum (PROWAG R302).
- **Slope:** Running slope of any multi-use pathway shall be no more than 5.0% unless the pathway is in a street or highway right of way. In this case, the grade shall not exceed the general grade of the adjacent street or highway (PROWAG R302.4, ADA 403.3 & WSDOT ADA Field Guide).
- **Heaving:** Heaving shall not exceed $\frac{1}{4}$ inch vertically except in the case of an additional $\frac{1}{4}$ inch that is beveled at a slope no greater than 1:2 (50%) (PROWAG R302, 2010 ADA Design Standards 302 & WSDOT ADA Field Guide). This includes holes, grates and other coverings as well with the long dimension of grate openings running perpendicular to the direction of travel. Additionally, grade breaks should be flush.
- **Obstruction:** Obstructions in the path include signs, poles, or other obstacles one might encounter. Non-pole-mounted objects with leading edges greater than 27 inches and less than 80 inches shall not protrude greater than 4 inches horizontally. This also implies that a vertical clear space of 80 inches minimum is required to pass under an object. Post-mounted objects shall overhang 4 inches maximum on either side when located between 27 inches and 80 inches from the ground. Obstructions shall not reduce the required 48-inch width of any pedestrian access route (PROWAG R402, WSDOT ADA Field Guide).

Railroad Crossings

Railroad crossings in the city of Cle Elum were assessed on their respective ADA ramps, the distance from the railroad which they lay, overall condition of the accessible route over the track, and the wheel flange width. Railroads were assessed in parts, one assessment for each corner



of the crossing. For a section of the crossing to pass, it had to meet all the criteria listed within the categories. There was only one railroad crossing in the city of Cle Elum to be evaluated on a pass/fail basis and it failed due to no ADA ramp and tactile warning surface.

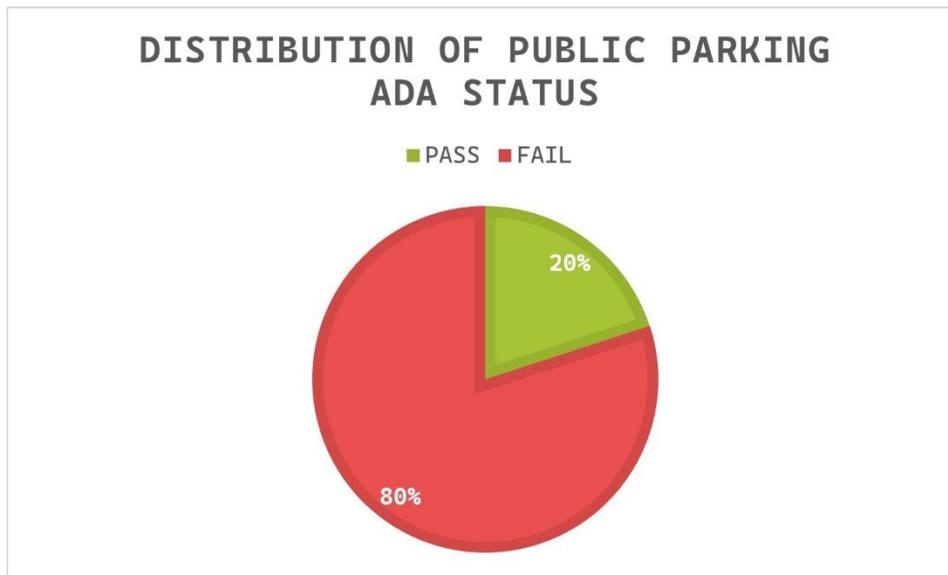


- **ADA Ramp:** At railroad crossings, ADA ramps shall be present when transitioning from sidewalk to railroad crossing. The ramp shall be compliant with ADA standards regarding width and slope, have no discontinuities, be aimed in the direction of travel, and equipped with a tactile warning surface (see Curb Ramps).
- **Distance From Ramp to Railroad:** The distance between the ADA ramp and the railroad shall be no shorter than 6 feet and no greater than 15 feet (WSDOT ADA Field Guide).
- **Condition of Accessible Route:** The condition of the accessible route between the two ADA ramps and over the railroad shall be ADA compliant. In terms of width, the route shall be no less than 48 inches. Cross slope shall be no greater than 2.0%, running slope no greater than 5.0%. Horizontal discontinuities shall be no greater than ½ inch and vertical shall be no greater than ¼ inch except in the case of an additional ¼ that is beveled at a slope no greater than 1:2. Obstructions in the walking path include signs, poles, or other obstacles one might encounter. Non-pole-mounted objects with leading edges greater than 27 inches and less than 80 inches shall not protrude greater than 4 inches horizontally. This also implies that a vertical clear space of 80 inches minimum is required to pass under an object. Post-mounted objects shall overhang 4 inches maximum on either side when located between 27 inches and 80 inches from the ground (PROWAG R402, ADA 303, ADA 307.3, ADA 403)
- **Wheel Flange Width:** The wheel flange width shall be no greater than 2½ inches (PROWAG R302.6.4, ADA 810.10)



Public-Accessible Parking

Public-accessible parking in the city of Cle Elum was assessed on five main categories including ADA parking symbol, ADA sign, slope of the parking space and accessible aisle, width of the parking space and accessible aisle, and presence of a compliant ADA curb ramp. For an accessible parking spot to pass it must meet all the criteria listed within the categories. Only two of the ten public parking spots passed in the city of Cle Elum. The cause of failure of the eight remaining spots were for several reasons including a failing curb ramp, width of access aisle, slope of access aisle and parking spot, and absence of signage.



- **ADA Parking Symbol:** An ADA parking symbol shall be painted within the accessible parking spot (WSDOT Standard Plan M-17.10-02). Additionally, the access aisle accompanying a parking spot shall be marked to discourage parking in the aisle (PROWAG R310.5).
- **ADA Sign:** An ADA parking sign shall be present when there are greater than 4 total parking spaces available. Additionally, for every six parking spaces, at least one shall be van accessible (PROWAG R310.5.5, ADA 208.2.4, ADA 502)
- **Slope:** The slope of either the parking space or the access aisle shall be no greater than 2.0% in any direction and shall not have any horizontal or vertical discontinuities (PROWAG R310.5.4, ADA 302, ADA 502).
- **Width and Length:** The width of any parking car parking space shall be 96 inches minimum. The width of any van parking space shall be 132 inches minimum except in the case when the access aisle is 96 inches minimum, then a van parking space shall be permitted to be 96 inches minimum. The width of any access aisle shall be 60 inches minimum. It is permitted that two car parking spaces shall share a common accessible aisle that is 60 inches unless one is a van parking space. If such is the case, the access aisle shall be 96 inches

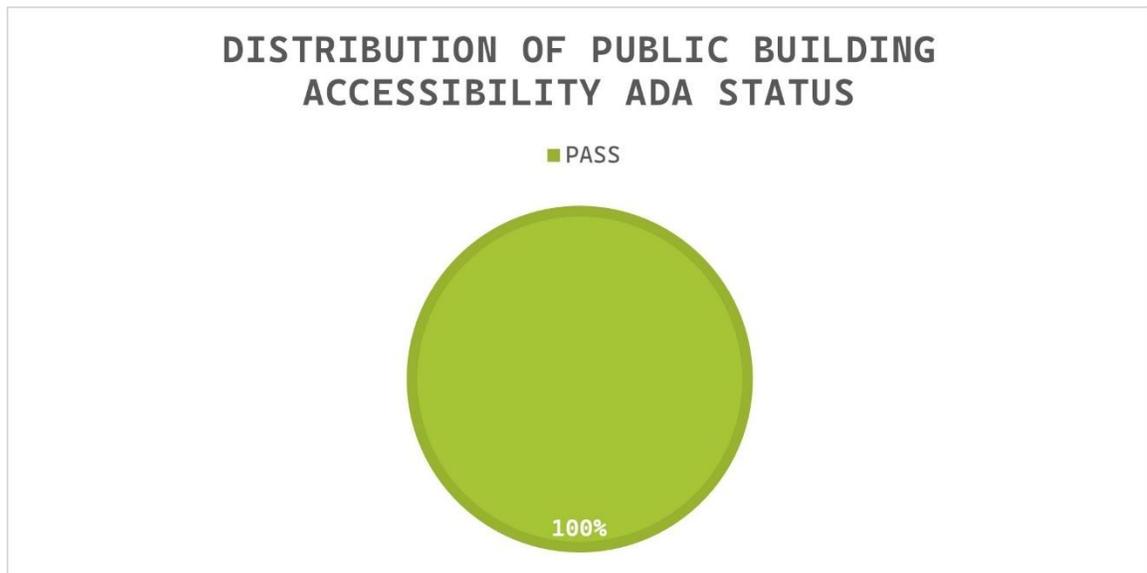


minimum. The length of any access aisle shall extend the full length of the parking spot it is serving (PROWAG R310, ADA 502).

- **Curb Ramp:** An ADA curb ramp shall accompany an accessible parking spot and access aisle. The curb ramp must meet the ADA standard listed above, except a curb ramp that is only serving on-street parking is not required to have a tactile warning surface (PROWAG R310.5.3). WSDOT Standard Plan M-17.10-02 includes tactile warning surface.

Access Approaches to Public Buildings

The access approaches to public buildings were assessed by the clear width of the entrance, the slope of the walkway leading to the entrance, and the maneuverable clearance provided to access the entrance. For an access approach to a public building to pass, it must have passed all the criteria listed below where applicable. All the access approaches to public buildings that were assessed in the city of Cle Elum passed the ADA criteria.



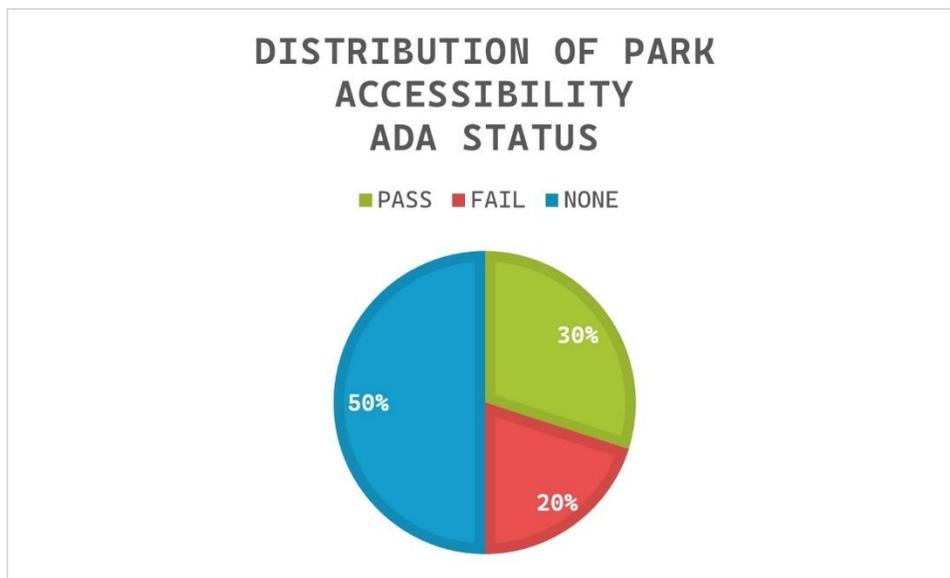
- **Clear Width:** The clear width shall be a minimum of 32 inches measuring from the face of the door to the stop, where the door is open 90 degrees (2010 ADA Design Standards 404.2.3).
- **Slope:** The slope of the walkway leading to the entrance shall have a cross slope no greater than 2.0% and a running slope no greater than 5.0% (PROWAG R302, 2010 ADA Design Standards 403.3). If a ramp is present, the ramp shall have a running slope no greater than 8.3% and a cross slope no greater than 2.0% (PROWAG R302, 2010 ADA Design Standards 405). The maximum rise for any ramp run shall be no greater than 30 inches (PROWAG R407, 2010 ADA Design Standards 405.6).
- **Maneuverable Clearance:** The maneuverable clearance shall extend the full width of the doorway and length of the clearance shall extend out 60 inches on the pull side and 48 inches on the push side (PROWAG R404, 2010 ADA Design



Standards 404.2.4). Within this clearance area, the slope in any direction shall not exceed 2.0% (PROWAG R404, 2010 ADA Design Standards 404.2.4.4).

Park Facilities

The park facilities were assessed by the accessible route leading to the play area or other park facility which can be broken into clear width, vertical clearance, slope, heaving, and obstructions. For a park facility to pass, it must have passed all the criteria listed below where applicable. None of the parks in the city of Cle Elum had an accessible route leading to a play area. Only City Park had a play area which did not have an accessible route leading to it, so that facility failed. Howard Carlin Memorial Trailhead had an accessible route which passed ADA standards. The remaining parks had no accessible routes to assess, so they were marked as None.



- **Clear Width:** At ground level, the clear width of an accessible route shall be a minimum of 60 inches except in the case of a play area that is less than 100 square feet which the clear area is permitted to be 48 inches minimum and at least one turning space is provided where the route extends past 30 feet. The clear width is also permitted to be 36 inches minimum for a maximum distance of 60 inches provided the reduced segments are separated by 60-inch-wide and 60-inch-long segments (PROWAG R302, 2010 ADA Design Standards 1008.2.4).
- **Vertical Clearance:** The vertical clearance of an accessible path at ground level shall be a minimum of 80 inches (PROWAG R302, 2010 ADA Design Standards 1008.2).
- **Slope:** The running slope shall not exceed 8.3% and the cross slope shall not exceed 2.0% (PROWAG R302, 2010 ADA Design Standards 403).
- **Heaving:** Heaving shall not exceed ¼ inch vertically except in the case of an additional ¼ inch that is beveled at a slope no greater than 1:2 (50%) (PROWAG R302, 2010 ADA Design Standards 302 & WSDOT ADA Field Guide). This

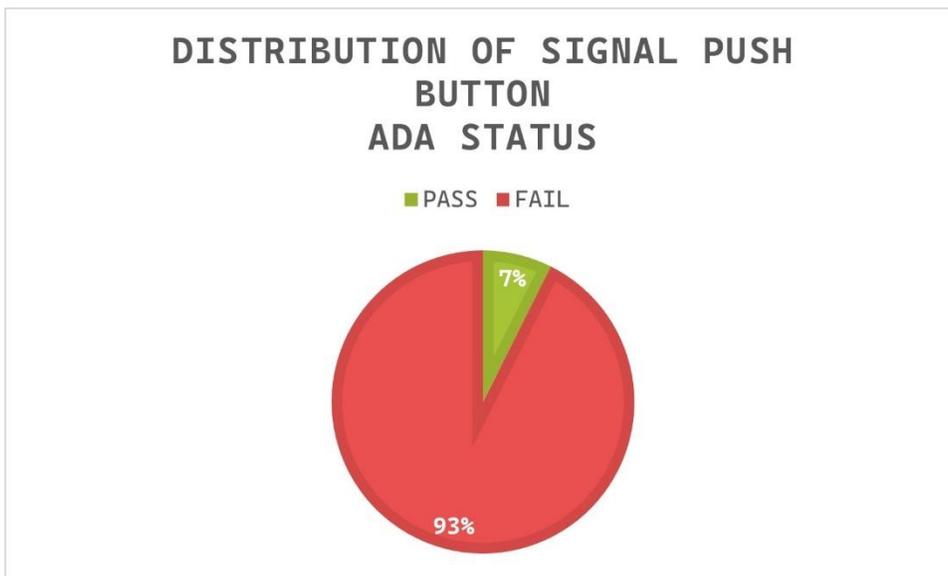


includes holes, grates and other coverings as well with the long dimension of grate openings running perpendicular to the direction of travel.

- **Obstruction:** Obstructions in the walking path include signs, poles, or other obstacles one might encounter. Non-pole-mounted objects with leading edges greater than 27 inches and less than 80 inches shall not protrude greater than 4 inches horizontally. This also implies that a vertical clear space of 80 inches minimum is required to pass under an object. Post-mounted objects shall overhang 4 inches maximum on either side when located between 27 inches and 80 inches from the ground. Obstructions shall not reduce the required 48-inch width of any pedestrian access route (PROWAG R402, WSDOT ADA Field Guide).

Signal Pushbuttons

APS pushbuttons were assessed on the same criteria regardless of whether they were for traffic lights or rapid flashing beacons. The main categories include the assembly, audio, vibration, and location of the button and housing. For an APS pushbutton to pass, it must meet all the requirements listed. All the APS pushbuttons in the city of Cle Elum failed for a variety of reasons listed within the categories.



- **Assembly:** The assembly of the button and housing mainly consists of the control face which shall be parallel to the crosswalk in which it is serving, and the button itself shall have a tactile arrow pointing in the direction of the crosswalk. The button shall also provide a sharp contrast against the housing (PROWAG R307, WSDOT ADA Field Guide).
- **Audio:** There must be an audible sound indicating the “walk” period as well as an audible sound as a locator tone during the “don’t walk” period (PROWAG R307 & R308).



- **Vibration:** APS pushbuttons shall also give off a vibration to indicate the walk interval (PROWAG R307 & R308).
- **Location:** Finally, the location of the APS pushbutton is critical. The button shall be within 1½ feet and 10 feet from the curb and no greater than 5 feet from the extended crosswalk line and curb ramp. When mounting the APS pushbutton, it shall be mounted between 15 and 48 inches from the ground. Clear space shall be provided, within reach range of the APS pushbutton, which connects to the crosswalk via sidewalk and/or curb ramp. Clear space shall have maximum running and cross slopes of 2.0%. The reach range shall be no more than 10 inches from the clear space to the pushbutton if it is mounted between 46 to 48 inches, otherwise reach range shall be no more than 24 inches. If more than one APS pushbutton is present at a corner, they must be 10 feet apart minimum (PROWAG R307, WSDOT ADA Field Guide).

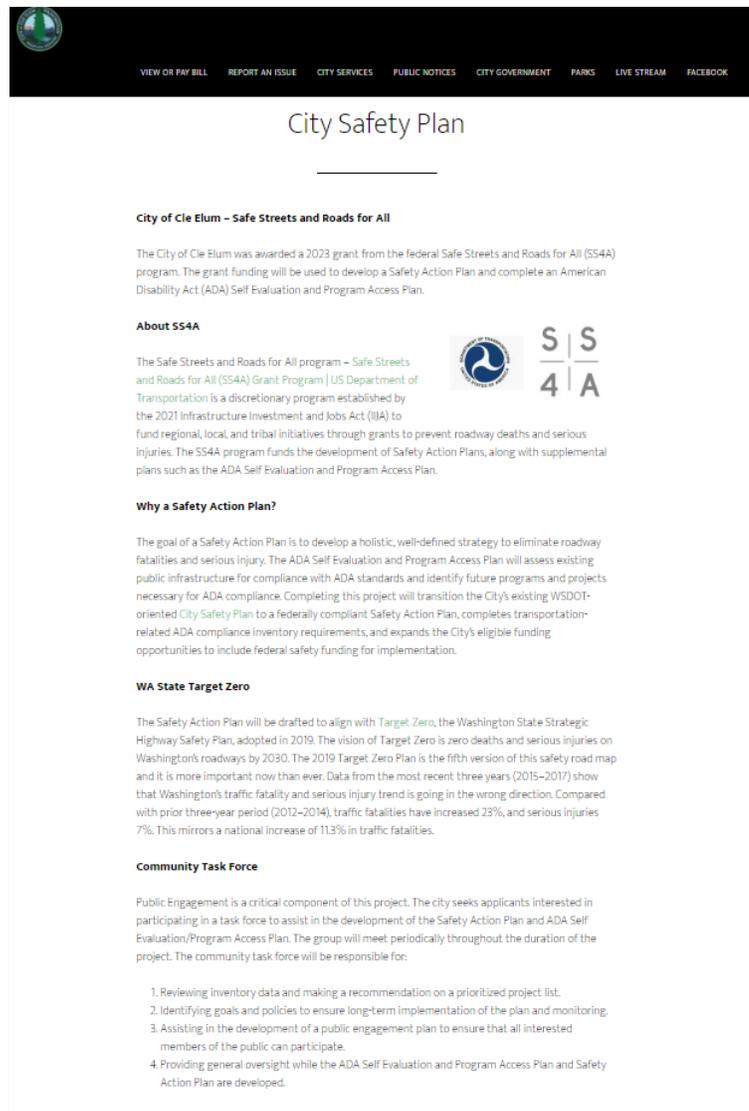


CHAPTER 3 – STAKEHOLDER ENGAGEMENT

Public and stakeholder input is an essential element in the transition plan development and self-evaluation processes. ADA implementation regulations require public entities to provide an opportunity to interested persons, including individuals with disabilities or organizations representing individuals with disabilities, to participate in the self-evaluation process and development of the transition plan by submitting comments (28 CFR 35, 105(b) and 28 CFR 35.150(d)(1)).

There were three primary goals for the public outreach activities prior to adopting the plan:

- Inform the public about the City’s plan and processes regarding removal of barriers to accessibility within the right-of-way. Provide information to assist interested parties to understand the issues faced by the City, alternatives considered and planned actions.
- Obtain public comment to identify any errors or gaps in the proposed accessibility transition plan for the public rights-of-way, specifically on prioritization and grievance processes.
- Meet Title II requirements for public comment opportunity.



3.1 ENGAGEMENT METHODS

To generate public involvement and capture public feedback on the ADA Transition Plan, the City used several methods:

3.1.1 Project Webpage

A [project webpage](#) was developed to inform residents of the project and to upload new information when made available. The webpage includes information on the federal SS4A grant that funded the plan, alignment with WA State Target Zero Goals, and ways the public can participate.



3.1.2 Public Comment

The draft ADA Transition Plan was made available for public comment on October XX, 2025. Submitted comments are included in Appendix B.

3.2 PROCESS FOR REQUESTING ACCOMMODATION

Many people who are blind or have low vision, who are deaf or hard of hearing, or who have other communication disabilities are prevented from participating fully in programs, activities, and services because of communication issues. To address this, Title II requires that communication with people with disabilities be as effective as communication with others. Oftentimes, good communication practices will suffice, such as not turning away when speaking with a person who is hard of hearing or taking the time to listen to a person who has a speech impairment. Other times a public entity needs to provide “auxiliary aids and services” to assure effective communication.

Requests for accommodation can be submitted to Rob Omans, City Administrator.



CHAPTER 4 – BARRIER REMOVAL METHODS AND SCHEDULE

Chapter 4 provides a summary of barrier removal methods and priorities to guide implementation of this plan. This chapter presents a total planning level cost estimate for the removal of existing pedestrian barriers. Finally, a schedule is presented that outlines the steps necessary to achieve compliance with current ADA standards.

4.1 BARRIER REMOVAL METHODS – PUBLIC ROW

Cle Elum has outlined methods for the removal of non-compliant ADA facilities in the public right-of-way.

4.1.1 Planning Level Cost Estimates to Remove Pedestrian Barriers

Unit Prices

Construction costs for upgrading facilities can vary depending on each individual improvement and conditions of each site. Costs can also vary on the type and size of project the improvements are associated with. Listed below are representative cost estimates for some typical accessibility improvements based on if the improvements are included as part of a stand-alone retrofit type project, or as part of a larger comprehensive capital improvement project.

Facility	Stand-Alone Project	Part of Capital Project
Curb Ramp	+/- \$5,500	+/- \$3,000
APS Signal	+/- \$30,000	+/- \$19,000
Sidewalk or Multi-Use Pathway	+/- \$16.00 / SF	+/- \$8.00 / SF
Crosswalk		+/- \$2,000
ADA Parking		+/- \$2,500
Bus Stop	\$600	\$375
Railroad Crossing	+/- \$250,000	+/- \$100,000
Public Building or Park Access	+/- \$16.00 / SF	+/- \$8.00 / SF

4.1.2 Planned Capital Improvement Projects

Cle Elum’s 6-Year Transportation Improvement Program (TIP) 2026-2031 was adopted on June 24, 2025. The TIP includes 17 projects, three of which have funding secured, the remaining are planned projects. Included within the secured and planned projects are 10 projects that have ADA components in the description, including, but not limited to: general ADA improvements, new sidewalk, roundabout infrastructure, and pedestrian safety improvements.



ADA SELF-EVALUATION AND TRANSITION PLAN



Six Year Transportation Improvement Program From 2026 to 2031

Agency: Cle Elum
County: Kittitas

MPO/RTPO: Quad-Co RTP0

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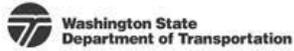
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Terminals F. Project Description	B. STIP ID	Hearing	A. Adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
09	1	<p>WA-10078</p> <p>Railroad Street Rehabilitation and Truck Route</p> <p>Pennsylvania Avenue to Peoh Avenue and First Street</p> <p>Full depth reclamation for truck route, curb and gutter, sidewalk, storm drainage, pavement markings, and ADA curb ramps. Truck route to preserve First Street downtown revitalization. From Pennsylvania Avenue to Harris Avenue, improvements only include curb and sidewalks on north side.</p>	WA-10078	06/23/25	06/23/25		2025-015	04	C G P S T W	0.400	CE	No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	PE	2026	Discretionary	209,500		0	32,700	242,200
P	CN	2027	Discretionary	1,286,900		0	200,900	1,487,800
Totals				1,496,400		0	233,600	1,730,000

Expenditure Schedule						
Phase	1st	2nd	3rd	4th	5th & 6th	
PE	209,500	0	0	0	0	0
CN	0	1,487,800	0	0	0	0
Totals	209,500	1,487,800	0	0	0	0



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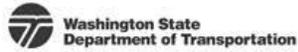
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Terminals F. Project Description	B. STIP ID	Hearing	A. Applied	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
			G. Structure ID									
07	2	Signalized Intersection Safety Improvements at W First St and Hartwig Blvd W First St and Hartwig Blvd W First St to Hartwig Blvd Install signal upgrades including protected/permitted left turn phasing, radar detection, visibility enhancements, storm drainage, ADA upgrades, and miscellaneous improvements.	WA-16322	06/23/25	06/23/25		2025-015	21		0.010	CE	No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
S	CN	2026	HSIP	293,000		0	0	293,000
Totals				293,000		0	0	293,000

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
CN	293,000	0	0	0	0
Totals	293,000	0	0	0	0



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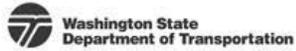
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Terminals F. Project Description	B. STIP ID G. Structure ID	Hearing	Adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
06	3	Stafford Avenue Sidewalk and SR 903 Speed Camera Stafford Avenue Second Street to Third Street Project will construct sidewalk on the east side of Stafford Avenue which has no pedestrian facilities north of Second Street. In addition to the sidewalk project, a speed camera with photo enforcement capabilities will help enforce the speed limit on SR 903 in the vicinity of Cle Elum-Roslyn schools. Project is fully funded with federal funds utilizing Toll Credits for local match.	WA-15855	06/23/25	06/23/25		2025-015	28		0.050	CE	No

Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
S	CN	2026	TA(R)	161,470		0	0	161,470
Totals				161,470		0	0	161,470

Phase	1st	2nd	3rd	4th	5th & 6th
CN	161,470	0	0	0	0
Totals	161,470	0	0	0	0



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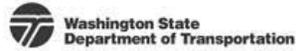
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Termini F. Project Description	B. STIP ID G. Structure ID	Hearing	Adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
05	4	First Street and Oakes Avenue Resurfacing First Street and Oakes Avenue Oakes Avenue to Peoh Avenue Mill and overlay on First Street from Oakes Avenue to Peoh Avenue, and along Oakes Avenue from First Street to Second Street, and pavement markings.	WA-16347	06/23/25	06/23/25		2025-015	05		0.500	CE	No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
S	CN	2026	STB G(R)	622,214		0	97,108	719,322
Totals				622,214		0	97,108	719,322

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
CN	719,322	0	0	0	0
Totals	719,322	0	0	0	0



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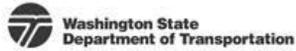
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Termini F. Project Description	B. STIP ID	Hearing	Approved	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
06	5	Chipseal, major street locations; 4-year cycles to Chipseal and sweep at the following locations: First Street Second Street Third Street Stafford Ave/Hartwig Blvd. Oakles Ave. Montgomery Ave. Columbia Ave.	WA-08106	06/23/25	06/23/25		2025-016	06		5.300	CE	No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	ALL	2026		0		0	259,500	259,500
Totals				0		0	259,500	259,500

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
ALL	259,500	0	0	0	0
Totals	259,500	0	0	0	0



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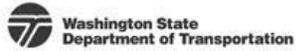
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Terminals F. Project Description	B. STIP ID G. Structure ID	Hearing	A. Adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
D7	6	Citywide Active Transportation Connector Second Street Wright Avenue to Montgomery Avenue Multi use pedestrian pathway	WA-14342	06/23/25	06/23/25		2025-015	28	C G P S T W	0.300	CE	No

Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	PE	2026	Discretionary	10,000		0	0	10,000
P	CN	2027	Discretionary	1,090,000		0	0	1,090,000
Totals				1,100,000		0	0	1,100,000

Phase	1st	2nd	3rd	4th	5th & 6th
PE	10,000	0	0	0	0
CN	0	1,090,000	0	0	0
Totals	10,000	1,090,000	0	0	0



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Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Termini F. Project Description	B. STIP ID	Hearing	Adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
05	7	Rostyn CR-W/ First Street Bridge Replacement Stafford Avenue to End of Bridge Full replacement of the First Street Bridge over Crystal (Rostyn) Creek including two vehicular travel lanes, sidewalks on both sides, and single-slope concrete barriers to improve safety and ADA accessibility.	G. Structure ID WA-18524	06/23/25	06/23/25		2025-015	11		0.010		No

Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	PE	2026	FBP	421,850		0	0	421,850
P	CN	2027	FBP	2,580,500		0	0	2,580,500
Totals				3,002,350		0	0	3,002,350

Phase	1st	2nd	3rd	4th	5th & 6th
PE	421,850	0	0	0	0
CN	0	2,580,500	0	0	0
Totals	421,850	2,580,500	0	0	0



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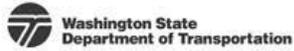
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Termini F. Project Description	B. STIP ID	Hearing	A. adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
06	8	Pine Street and First Street Intersection Improvements to Construct a roundabout at the intersection of Pine Street and First Street.	WA-13584	06/23/25	06/23/25		2025-016	04	C G P S T W		CE	No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	ALL	2026		0		0	970,000	970,000
Totals				0		0	970,000	970,000

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
ALL	970,000	0	0	0	0
Totals	970,000	0	0	0	0



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Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Termini F. Project Description	B. STIP ID	Hearing	A. Applied	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
06	9	Stafford Avenue Sidewalks Stafford Avenue Third Street to Reed Street Construct new sidewalk on the west side of the road.	WA-16499	06/23/25	06/23/25		2025-015	28				No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	PE	2026		0	T1B	24,200	0	24,200
P	CN	2027		0	T1B	205,800	0	205,800
Totals				0		230,000	0	230,000

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
PE	24,200	0	0	0	0
CN	0	205,800	0	0	0
Totals	24,200	205,800	0	0	0



ADA SELF-EVALUATION AND TRANSITION PLAN



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Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Termini F. Project Description	B. STIP ID G. Structure ID	Hearing	Adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
D9	10	Chipseal, local access streets; 8-year cycles to Chipseal and sweep the following locations: Deer Meadow Dr. Cottage Ave. Garden St. Short Ave. Floral Ave. Kerman Ave. Yakima Ave. Teanaway Ave. Peoh Ave. Bullitt Ave. Wright Ave. Harris Ave. Pennsylvania Ave. Billings Ave. Rossetti Way/Reed St. Shober Way Roslyn Place Alpha Way Stewart Mew Dr. Ranger Station Rd. Douglas Munro Blvd. Reed St. Steiner St. Park St. Sixth St. Fifth St. Fourth St. Railroad Ave. Third St. Grant Ave. Washington St. Broadway Ave. Lincoln Ave. Cleveland Ave. Madison St.	WA-08102	06/23/25	06/23/25		2025-015	05		10.820	CE	No



ADA SELF-EVALUATION AND TRANSITION PLAN



Six Year Transportation Improvement Program From 2026 to 2031

Agency: Cle Elum

County: Kittitas

MPO/RTPO: Quad-Co RTP O

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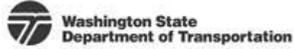
Y Outside

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	ALL	2027		0		0	540,000	540,000
Totals				0		0	540,000	540,000

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
ALL	0	540,000	0	0	0
Totals	0	540,000	0	0	0



ADA SELF-EVALUATION AND TRANSITION PLAN



Six Year Transportation Improvement Program From 2026 to 2031

Agency: Cle Elum

County: Kittitas

MPO/RTPO: Quad-Co RTPD

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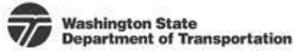
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Terminals F. Project Description	B. STIP ID G. Structure ID	Hearing	A. Applied	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
07	11	Second Street Resurfacing Reed Street to Pennsylvania Ave. 2-inch grind and overlay	WA-15890	06/23/25	06/23/25		2025-015	05		0.090		No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	ALL	2027	STBG	450,000		0	0	450,000
Totals				450,000		0	0	450,000

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
ALL	0	450,000	0	0	0
Totals	0	450,000	0	0	0



ADA SELF-EVALUATION AND TRANSITION PLAN



Six Year Transportation Improvement Program From 2026 to 2031

Agency: Cle Elum

County: Kittitas

MPO/RTPO: Quad-Co RTPD

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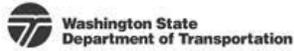
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Terminals F. Project Description	B. STIP ID	Hearing	A. Applied	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
07	12	Second Street Resurfacing Pennsylvania Ave. to Short Ave. 2-inch grind and overlay	WA-15177	06/23/25	06/23/25		2025-015	05		1.060	CE	No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	ALL	2027		0	T1B	1,045,000	55,000	1,100,000
Totals				0		1,045,000	55,000	1,100,000

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
ALL	0	1,100,000	0	0	0
Totals	0	1,100,000	0	0	0



ADA SELF-EVALUATION AND TRANSITION PLAN



Six Year Transportation Improvement Program From 2026 to 2031

Agency: Cle Elum

County: Kittitas

MPO/RTPO: Quad-Co RTPD

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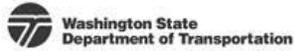
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Termini F. Project Description	B. STIP ID	Hearing	A. Adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
06	13	Ranger Station Road and Miller Avenue and Second Street Intersection Improvements to Construct a roundabout at the intersection of Ranger Station Road and Miller Avenue and Second Street.	WA-13582	06/23/25	06/23/25		2025-016	01	C G P S T W		CE	No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	ALL	2028		0		0	1,778,340	1,778,340
Totals				0		0	1,778,340	1,778,340

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
ALL	0	0	1,778,340	0	0
Totals	0	0	1,778,340	0	0



ADA SELF-EVALUATION AND TRANSITION PLAN



Six Year Transportation Improvement Program From 2026 to 2031

Agency: Cle Elum

County: Kittitas

MPO/RTPO: Quad-Co RTPD

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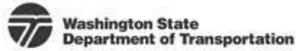
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Termini F. Project Description	B. STIP ID	Hearing	Approved	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
09	14	Railroad Street Truck Route Extension Railroad Street Peoh Avenue to Columbia Avenue Construct roadway extension for truck route including roadside swales.	WA-12020	06/23/25	06/23/25		2025-015	01	C G P S T W	0.480	CE	Yes

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	PE	2028		0		0	170,000	170,000
P	RW	2029		0		0	140,400	140,400
P	CN	2030		0		0	1,545,000	1,545,000
Totals				0		0	1,855,400	1,855,400

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
PE	0	0	170,000	0	0
RW	0	0	0	140,400	0
CN	0	0	0	0	1,545,000
Totals	0	0	170,000	140,400	1,545,000



ADA SELF-EVALUATION AND TRANSITION PLAN



Six Year Transportation Improvement Program From 2026 to 2031

Agency: Cle Elum

County: Kittitas

MPO/RTPO: Quad-Co RTP 0

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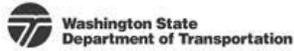
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Terminals F. Project Description	B. STIP ID G. Structure ID	Hearing	A. Adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
D7	15	Second Street Pathway - Phase 3 Second Street Montgomery Avenue to Short Avenue Multi use pedestrian pathway	WA-14343	06/23/25	06/23/25		2025-015	28	C G P S T W	0.590	CE	No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	PE	2029	Discretionary	10,000		0	0	10,000
P	CN	2029	Discretionary	1,490,000		0	0	1,490,000
Totals				1,500,000		0	0	1,500,000

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
PE	0	0	0	10,000	0
CN	0	0	0	1,490,000	0
Totals	0	0	0	1,500,000	0



ADA SELF-EVALUATION AND TRANSITION PLAN



Six Year Transportation Improvement Program From 2026 to 2031

Agency: Cle Elum

County: Kittitas

MPO/RTPO: Quad-Co RTPD

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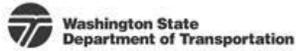
Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Termini F. Project Description	B. STIP ID	Hearing	A. Adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
07	16	Third Street Reconstruction Third Street Stafford Avenue to Short Avenue Full depth reclamation of the roadway.	WA-16501	06/23/25	06/23/25		2025-015	05		1.350		No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	ALL	2031		0	TIB	3,851,000	0	3,851,000
Totals				0		3,851,000	0	3,851,000

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
ALL	0	0	0	0	3,851,000
Totals	0	0	0	0	3,851,000



ADA SELF-EVALUATION AND TRANSITION PLAN



Six Year Transportation Improvement Program From 2026 to 2031

Agency: Cle Elum

County: Kittitas

MPO/RTPO: Quad-Co RTP 0

N Inside

Y Outside

Functional Class	Priority Number	A. PIN/Project No. C. Project Title D. Road Name or Number E. Begin & End Terminals F. Project Description	B. STIP ID G. Structure ID	Hearing	A. Adopted	Amendment	Resolution No.	Improvement Type	Utility Codes	Total Length	Environmental Type	R/W Required
D5	17	Pine Street and Second Street Intersection Improvements to Construction a roundabout at the intersection of Pine Street and Second Street.	WA-16525	06/23/25	06/23/25		2025-015	D1	C G P S T W			No

Funding								
Status	Phase	Phase Start Year (YYYY)	Federal Fund Code	Federal Funds	State Fund Code	State Funds	Local Funds	Total Funds
P	ALL	2031		0		0	760,000	760,000
Totals				0		0	760,000	760,000

Expenditure Schedule					
Phase	1st	2nd	3rd	4th	5th & 6th
ALL	0	0	0	0	760,000
Totals	0	0	0	0	760,000

	Federal Funds	State Funds	Local Funds	Total Funds
Grand Totals for Cle Elum	8,625,234	5,126,000	6,548,948	20,300,182



4.1.3 Operations and Maintenance

Cle Elum adopted its Complete Streets Ordinance (CEMC Ch. 10.40) on August 9, 2016. The purpose statement reads:

“The City of Cle Elum shall, to the maximum extent practical, scope, plan, design, construct, operate and maintain appropriate facilities for the safe accommodation of pedestrians, bicyclists, transit users, motorists, emergency responders, freight, and users of all ages and abilities in all new construction, retrofit, or reconstruction projects. Through ongoing operations and maintenance, the city of Cle Elum shall identify cost-effective opportunities to include complete streets practices.”

4.1.4 Permitted Development

New private development projects in Cle Elum are required to install and/or upgrade public improvements when required by the municipal code and adopted construction standards.

4.1.5 Entire Jurisdiction

Based on the results of the self-evaluation, the estimated costs associated with providing ADA accessibility within the entire jurisdiction is between \$5.6 – \$11.2 million, depending on the project and how it may be packaged with a broader capital project. This amount signifies a significant investment that Cle Elum is committed to making in the upcoming years, depending on available funding. A systematic approach to providing accessibility will be taken in order to absorb the cost into the Cle Elum budget for improvements to the public right of way.

City Wide						
Inventory	Stand-Alone Project	Qty	Total	Part of Capital Project	Qty	Total
Curb Ramp (ea)	\$ 5,500.00	124	\$ 682,000.00	\$ 3,000.00	124	\$ 372,000.00
APS Signal (ea)	\$ 30,000.00	9	\$ 270,000.00	\$ 19,000.00	9	\$ 171,000.00
Sidewalk (s.f.)	\$ 16.00	523265	\$ 8,372,240.00	\$ 8.00	523265	\$ 4,186,120.00
Pathway (s.f.)	\$ 16.00		\$ -	\$ 8.00		\$ -
Crosswalk (ea)	\$ 2,000.00	151	\$ 302,000.00	\$ 2,000.00	151	\$ 302,000.00
ADA Parking (ea)	\$ 2,500.00	22	\$ 55,000.00	\$ 2,500.00	22	\$ 55,000.00
Park Access (s.f.)	\$ 16.00	50	\$ 800.00	\$ 8.00	50	\$ 400.00
RR Crossing (ea)	\$ 250,000.00	6	\$ 1,500,000.00	\$ 100,000.00	6	\$ 600,000.00
			\$ 11,182,040.00			\$ 5,686,520.00

4.1.6 Barrier Removal Funding

Potential Funding Sources

There are several potential local, state and federal funding sources available for ADA projects.

- [Community Development Block Grant \(CDBG\)](#)
- [Surface Transportation Block Grant Program \(STBG\)](#)
- [Transportation Improvement Board \(TIB\)](#)
- [WSDOT Active Transportation Funding](#)
 - Safe Routes to School (SRTS)
 - Pedestrian and Bicycle Program
 - Sandy Williams Connecting Communities Program



ADA SELF-EVALUATION AND TRANSITION PLAN

- [Recreation and Conservation Office \(RCO\)](#) – Park Facilities only
- [USDA Rural Development](#)



CHAPTER 5 – RECOMMENDATIONS AND NEXT STEPS

This chapter provides a set of recommendations intended to inform the implementation of this Transition Plan and ongoing removal of pedestrian barriers. Recommendations are not presented in priority order and represent short-term and long-term Transition Plan implementation work plan tasks.

5.1 PRIORITY AREAS

Based on the results of the self-evaluation, the estimate costs associated with eliminating accessibility barriers within the targeted priority areas are below. The cost estimates include two categories including stand-alone projects and implementation as part of a larger capital construction project.

Stand-alone project would consist of both individual city projects that only construct the noted improvements, along with private development projects that would require frontage improvements. For example, a new subdivision will require new ADA compliant facilities internally and on the frontage road. Larger capital projects such as a complete roadway re-build are able to benefit from reduced rates for larger quantities and extensive mobilization.

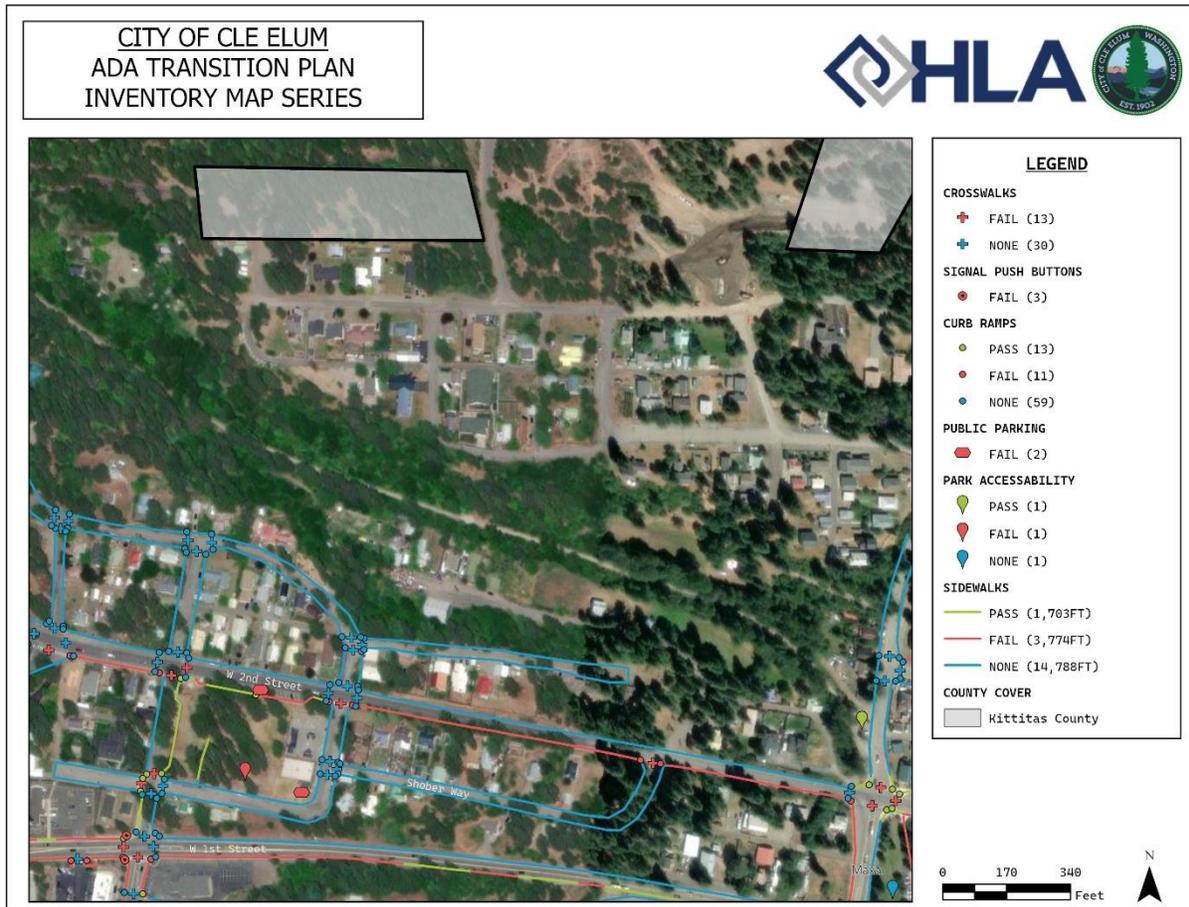
Cle Elum has identified specific locations as priority areas for planned accessibility improvement projects. These areas have been selected due to their proximity to specific land uses such as schools, government offices and medical facilities, as well as from the receipt of public comments. The priority areas as identified in the self-evaluation are as follows:



ADA SELF-EVALUATION AND TRANSITION PLAN

- West 2nd Street Vicinity

W 2nd St Vicinity						
Inventory	Stand-Alone Project	Qty	Total	Part of Capital Project	Qty	Total
Curb Ramp (ea)	\$ 5,500.00	70	\$ 385,000.00	\$ 3,000.00	70	\$ 210,000.00
APS Signal (ea)	\$ 30,000.00	3	\$ 90,000.00	\$ 19,000.00	3	\$ 57,000.00
Sidewalk (s.f.)	\$ 16.00	92810	\$ 1,484,960.00	\$ 8.00	92810	\$ 742,480.00
Pathway (s.f.)	\$ 16.00	0	\$ -	\$ 8.00	0	\$ -
Crosswalk (ea)	\$ 2,000.00	43	\$ 86,000.00	\$ 2,000.00	43	\$ 86,000.00
ADA Parking (ea)	\$ 2,500.00	0	\$ -	\$ 2,500.00	0	\$ -
Public Building (s.f.)	\$ 16.00	0	\$ -	\$ 8.00	0	\$ -
Park Access (s.f.)	\$ 16.00	50	\$ 800.00	\$ 8.00	50	\$ 400.00
Bus Stop (ea)	\$ 600.00	0	\$ -	\$ 375.00	0	\$ -
RR Crossing (ea)	\$ 250,000.00	0	\$ -	\$ 100,000.00	0	\$ -
			\$ 2,046,760.00			\$ 1,095,880.00

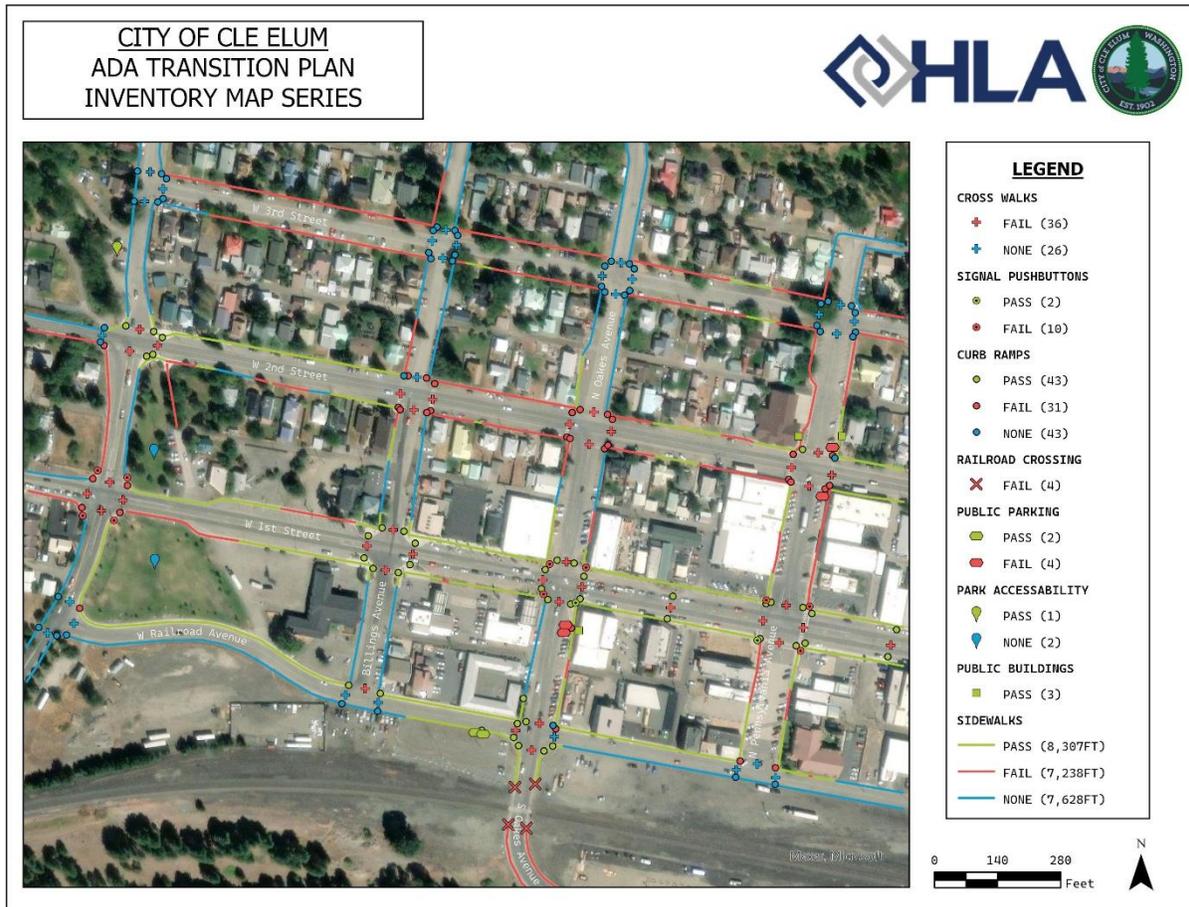




ADA SELF-EVALUATION AND TRANSITION PLAN

- Oakes Avenue Vicinity

Oakes Avenue Vicinity						
Inventory	Stand-Alone Project	Qty	Total	Part of Capital Project	Qty	Total
Curb Ramp (ea)	\$ 5,500.00	74	\$ 407,000.00	\$ 3,000.00	74	\$ 222,000.00
APS Signal (ea)	\$ 30,000.00	10	\$ 300,000.00	\$ 19,000.00	10	\$ 190,000.00
Sidewalk (s.f.)	\$ 16.00	74330	\$ 1,189,280.00	\$ 8.00	74330	\$ 594,640.00
Pathway (s.f.)	\$ 16.00	0	\$ -	\$ 8.00	0	\$ -
Crosswalk (ea)	\$ 2,000.00	62	\$ 124,000.00	\$ 2,000.00	62	\$ 124,000.00
ADA Parking (ea)	\$ 2,500.00	4	\$ 10,000.00	\$ 2,500.00	4	\$ 10,000.00
Public Building (s.f.)	\$ 16.00	0	\$ -	\$ 8.00	0	\$ -
Park Access (s.f.)	\$ 16.00	100	\$ 1,600.00	\$ 100.00	20	\$ 2,000.00
Bus Stop (ea)	\$ 600.00	9	\$ 5,400.00	\$ 375.00	9	\$ 3,375.00
RR Crossing (ea)	\$ 250,000.00	4	\$ 1,000,000.00	\$ 100,000.00	4	\$ 400,000.00
			\$ 3,037,280.00			\$ 1,546,015.00

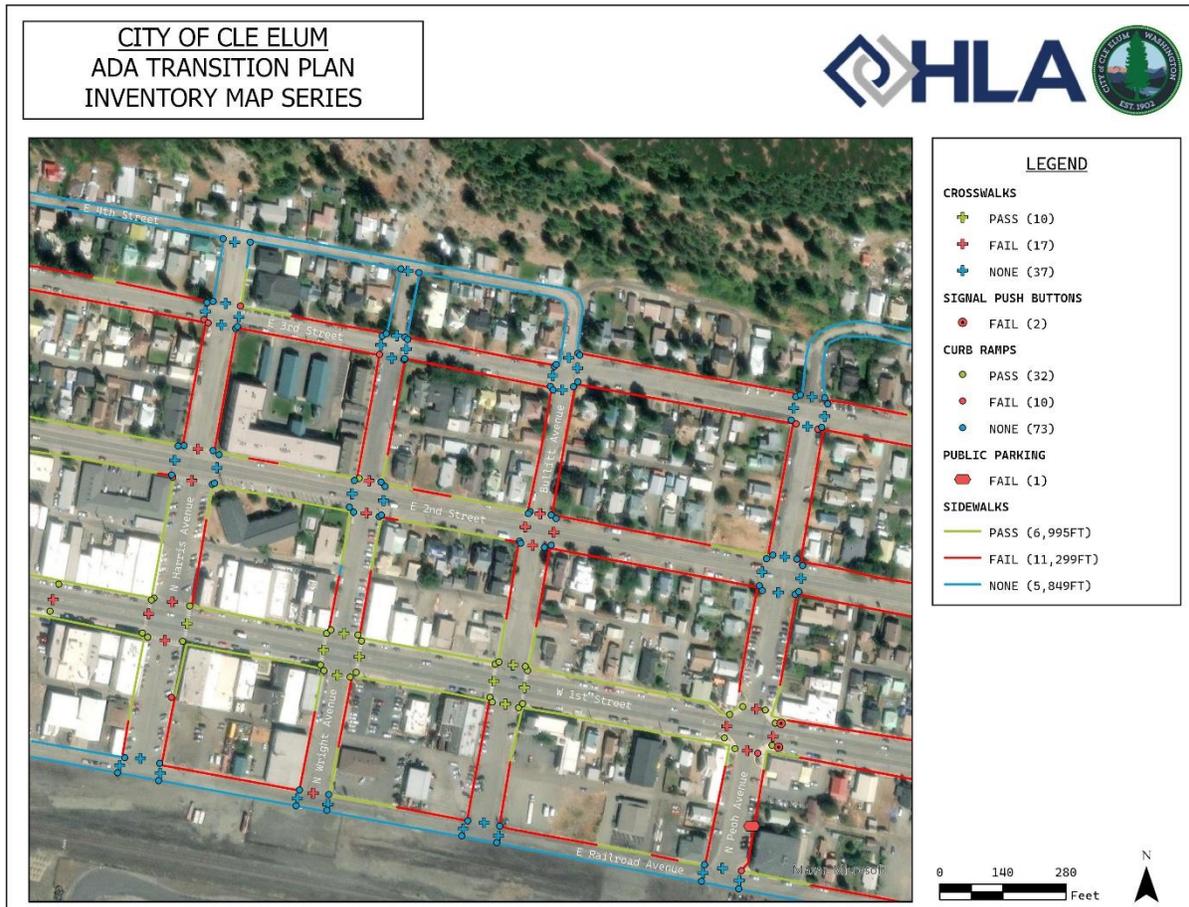




ADA SELF-EVALUATION AND TRANSITION PLAN

- Bullitt Avenue Vicinity

Bullitt Avenue Vicinity						
Inventory	Stand-Alone Project	Qty	Total	Part of Capital Project	Qty	Total
Curb Ramp (ea)	\$ 5,500.00	83	\$ 456,500.00	\$ 3,000.00	83	\$ 249,000.00
APS Signal (ea)	\$ 30,000.00	2	\$ 60,000.00	\$ 19,000.00	2	\$ 38,000.00
Sidewalk (s.f.)	\$ 16.00	85740	\$ 1,371,840.00	\$ 8.00	85740	\$ 685,920.00
Pathway (s.f.)	\$ 16.00	0	\$ -	\$ 8.00	0	\$ -
Crosswalk (ea)	\$ 2,000.00	54	\$ 108,000.00	\$ 2,000.00	54	\$ 108,000.00
ADA Parking (ea)	\$ 2,500.00	1	\$ 2,500.00	\$ 2,500.00	1	\$ 2,500.00
Public Building (s.f.)	\$ 16.00	0	\$ -	\$ 8.00	0	\$ -
Park Access (s.f.)	\$ 16.00	0	\$ -	\$ 8.00	0	\$ -
Bus Stop (ea)	\$ 600.00	0	\$ -	\$ 375.00	0	\$ -
RR Crossing (ea)	\$ 250,000.00	0	\$ -	\$ 100,000.00	0	\$ -
			\$ 1,998,840.00			\$ 1,083,420.00

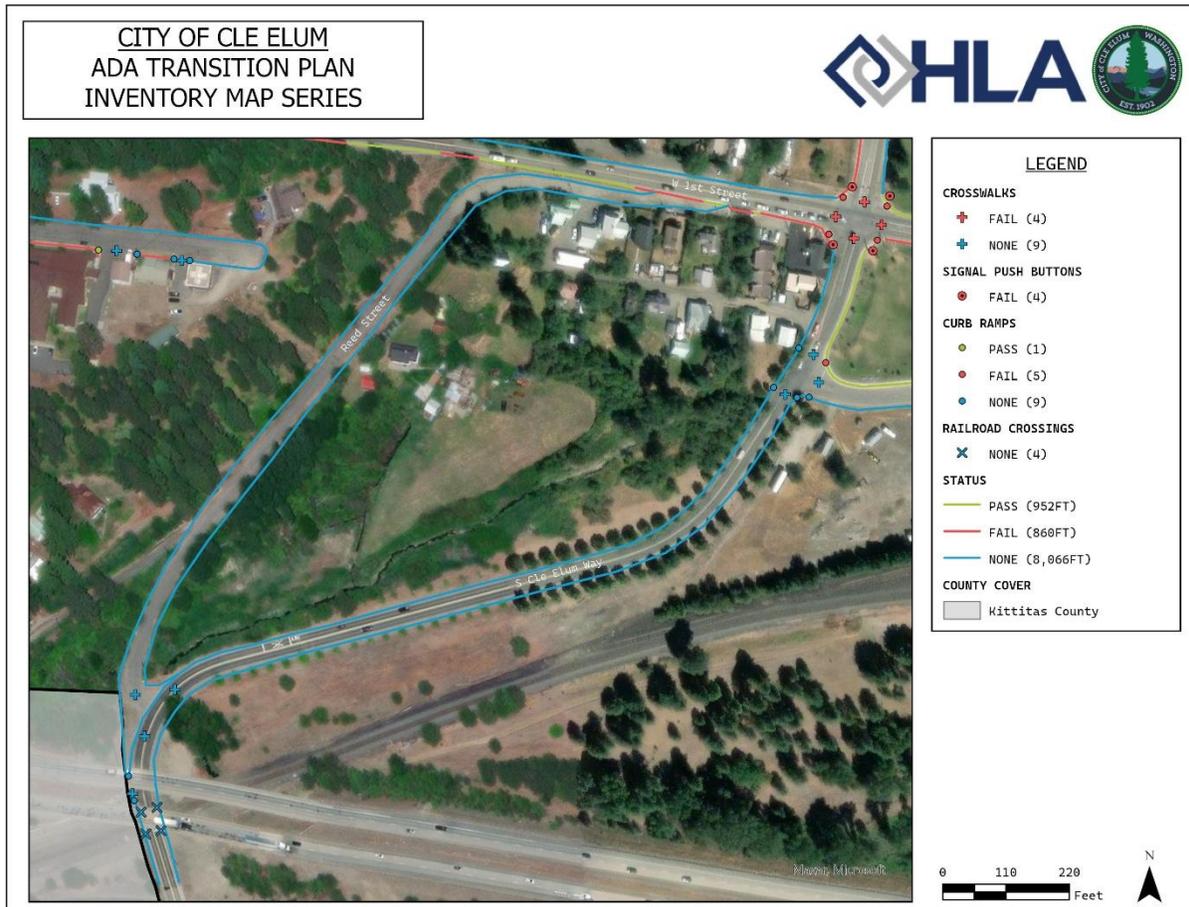




ADA SELF-EVALUATION AND TRANSITION PLAN

- South Cle Elum Way Vicinity

S Cle Elum Way Vicinity						
Inventory	Stand-Alone Project	Qty	Total	Part of Capital Project	Qty	Total
Curb Ramp (ea)	\$ 5,500.00	14	\$ 77,000.00	\$ 3,000.00	14	\$ 42,000.00
APS Signal (ea)	\$ 30,000.00	4	\$ 120,000.00	\$ 19,000.00	4	\$ 76,000.00
Sidewalk (s.f.)	\$ 16.00	44630	\$ 714,080.00	\$ 8.00	44630	\$ 357,040.00
Pathway (s.f.)	\$ 16.00	0	\$ -	\$ 8.00	0	\$ -
Crosswalk (ea)	\$ 2,000.00	13	\$ 26,000.00	\$ 2,000.00	13	\$ 26,000.00
ADA Parking (ea)	\$ 2,500.00	0	\$ -	\$ 2,500.00	0	\$ -
Public Building (s.f.)	\$ 16.00	0	\$ -	\$ 8.00	0	\$ -
Park Access (s.f.)	\$ 16.00	0	\$ -	\$ 8.00	0	\$ -
Bus Stop (ea)	\$ 600.00	0	\$ -	\$ 375.00	0	\$ -
RR Crossing (ea)	\$ 250,000.00	4	\$ 1,000,000.00	\$ 100,000.00	4	\$ 400,000.00
			\$ 1,937,080.00			\$ 901,040.00





5.2 EXTERNAL AGENCY COORDINATION

Other agencies are responsible for pedestrian facilities within and adjacent to the City of Cle Elum. The city will coordinate with Kittitas County and WSDOT, as necessary, to track and assist in the facilitation of the elimination of accessibility barriers along their routes.

5.3 SCHEDULE

Cle Elum has set the following schedule goals for improving the accessibility of its pedestrian facilities within the city's jurisdiction:

- ADA improvements will be made consistent with planned and ongoing street improvement projects, as amended annually in the 6-year Transportation Improvement Program (TIP).
- Additional improvements will be made in conjunction with new private projects that require frontage improvements and/or when additional funding opportunities come available.

5.4 METHODOLOGY

Cle Elum will utilize two methods for upgrading pedestrian facilities to the current ADA standards. The first and most comprehensive of the two methods are the scheduled street and utility improvement projects. All pedestrian facilities impacted by these projects will be upgraded to current ADA accessibility standards. The second method is the stand alone sidewalk and ADA accessibility improvement project. These projects will be incorporated into the 6-year TIP, on a case by case basis as determined by Cle Elum staff. The 6-year TIP, which includes a detailed schedule and budget for specific improvements, is included in Section 4.12



CHAPTER 6 Appendices

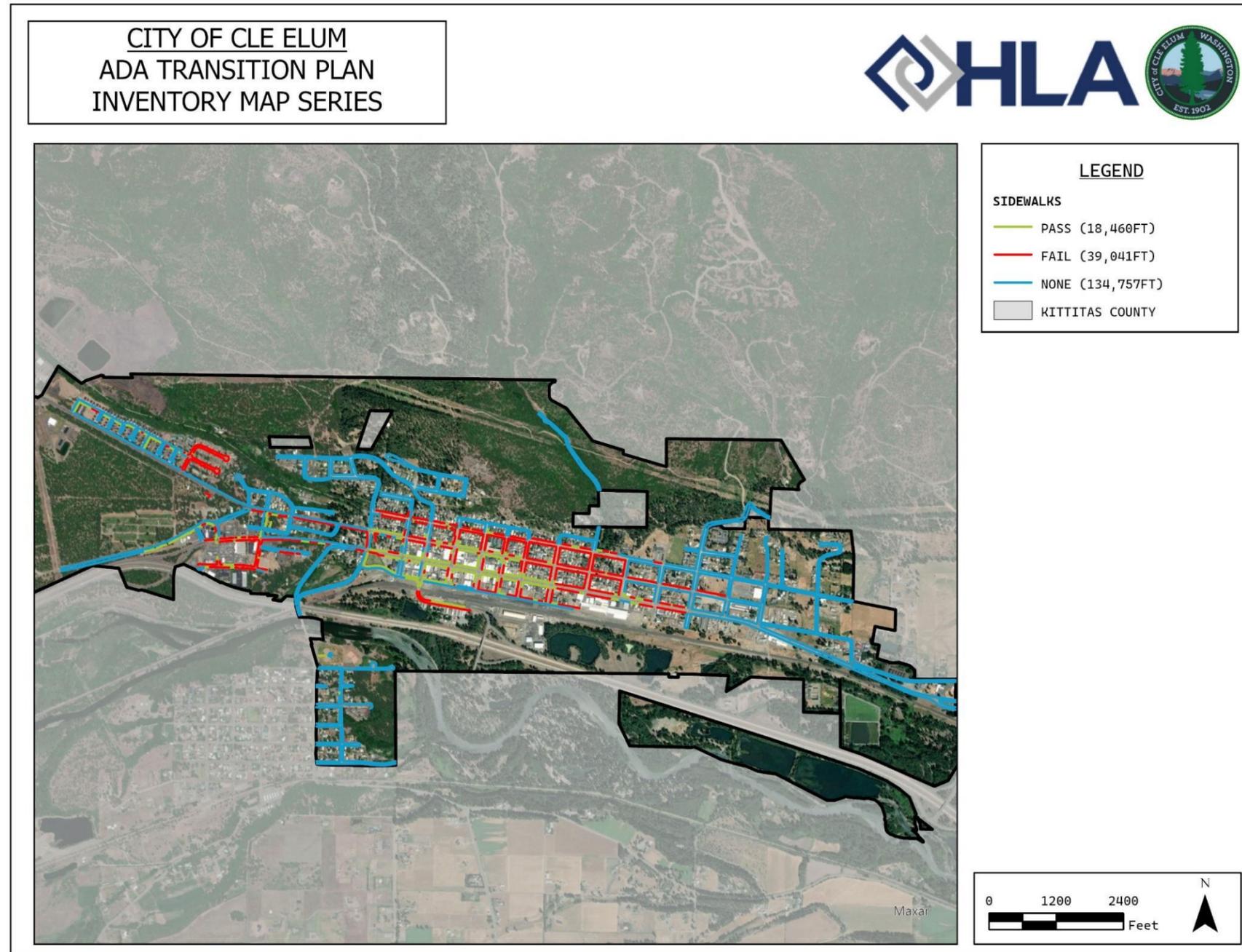
- A. Maps of ADA Compliance Inventory**
- B. Public Comment**
- C. Grievance Procedure**
- D. Contact Information**
- E. Agency ADA Design Standards and Procedures**
- F. Glossary of Terms**



ADA SELF-EVALUATION AND TRANSITION PLAN

A. MAPS OF ADA COMPLIANCE INVENTORY

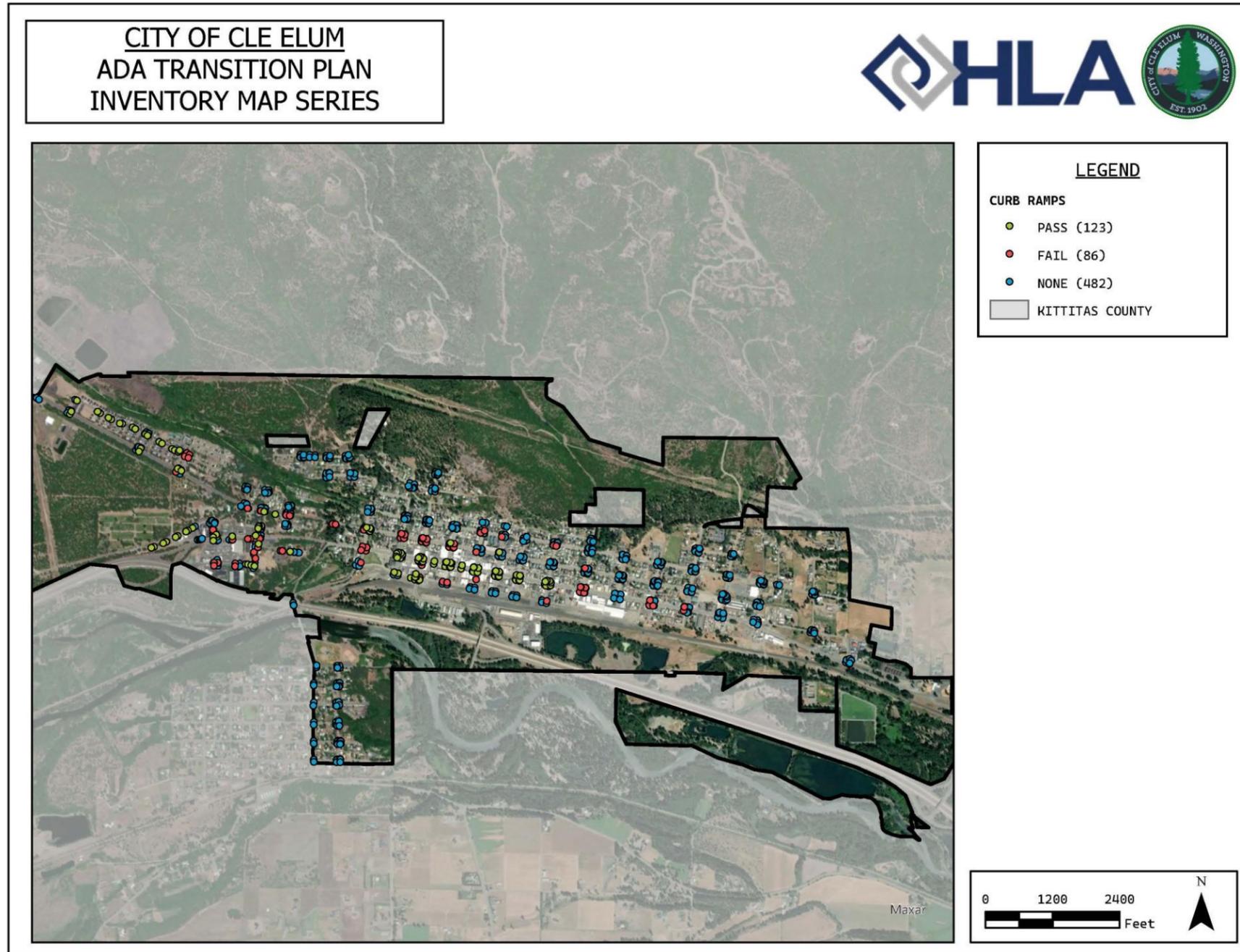
Sidewalk





ADA SELF-EVALUATION AND TRANSITION PLAN

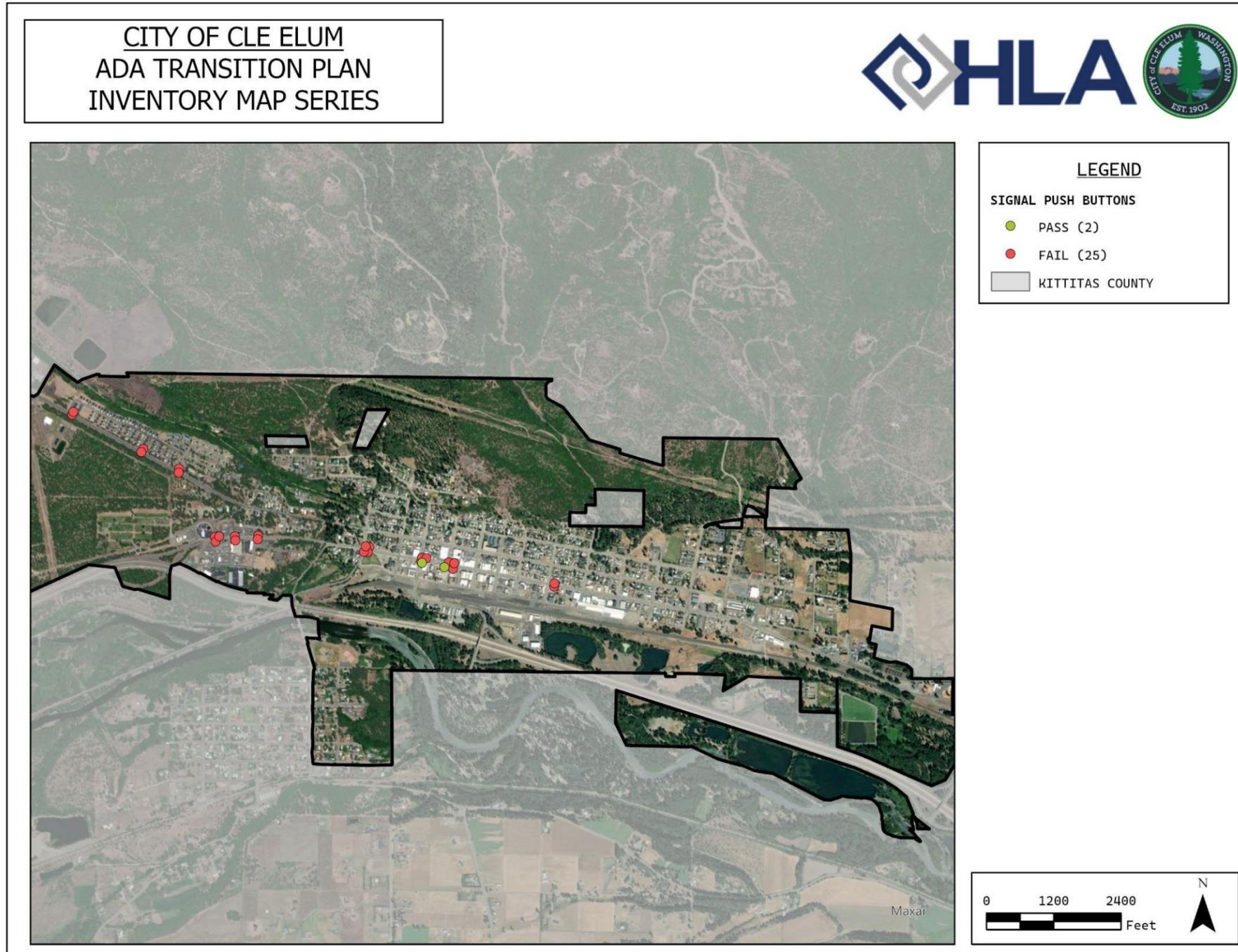
Curb Ramps





ADA SELF-EVALUATION AND
TRANSITION PLAN

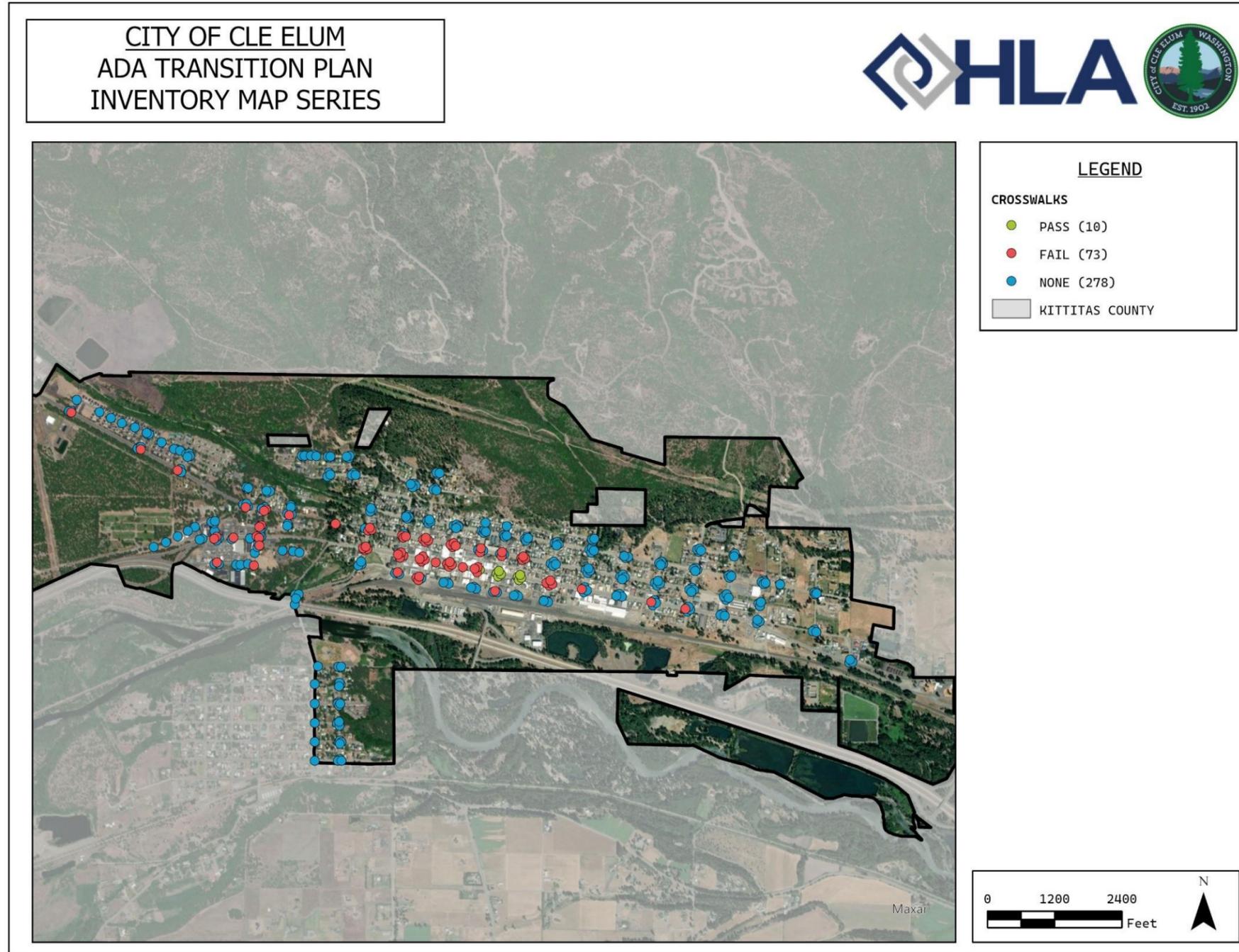
Crosswalk Pushbutton





ADA SELF-EVALUATION AND
TRANSITION PLAN

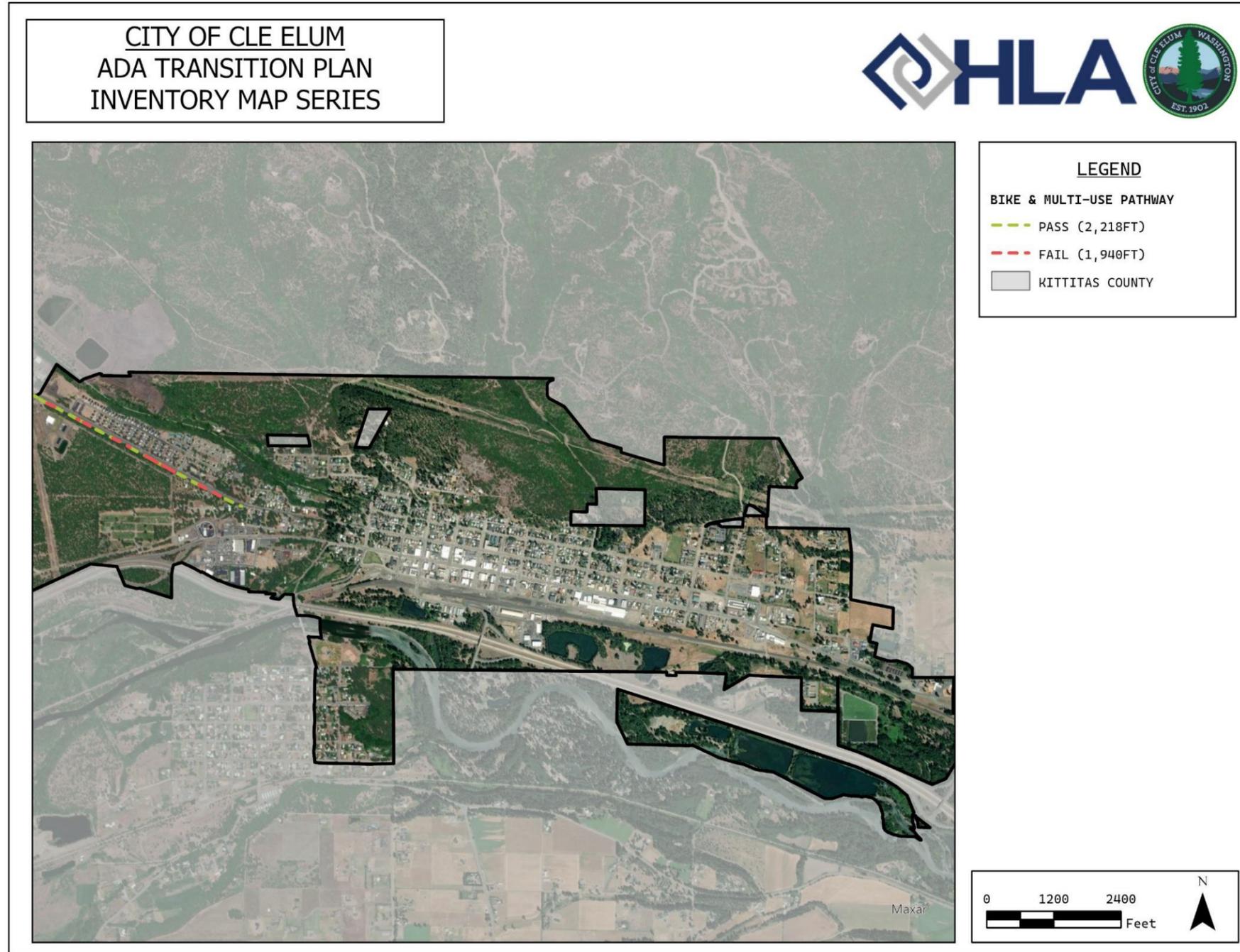
Crosswalks





ADA SELF-EVALUATION AND
TRANSITION PLAN

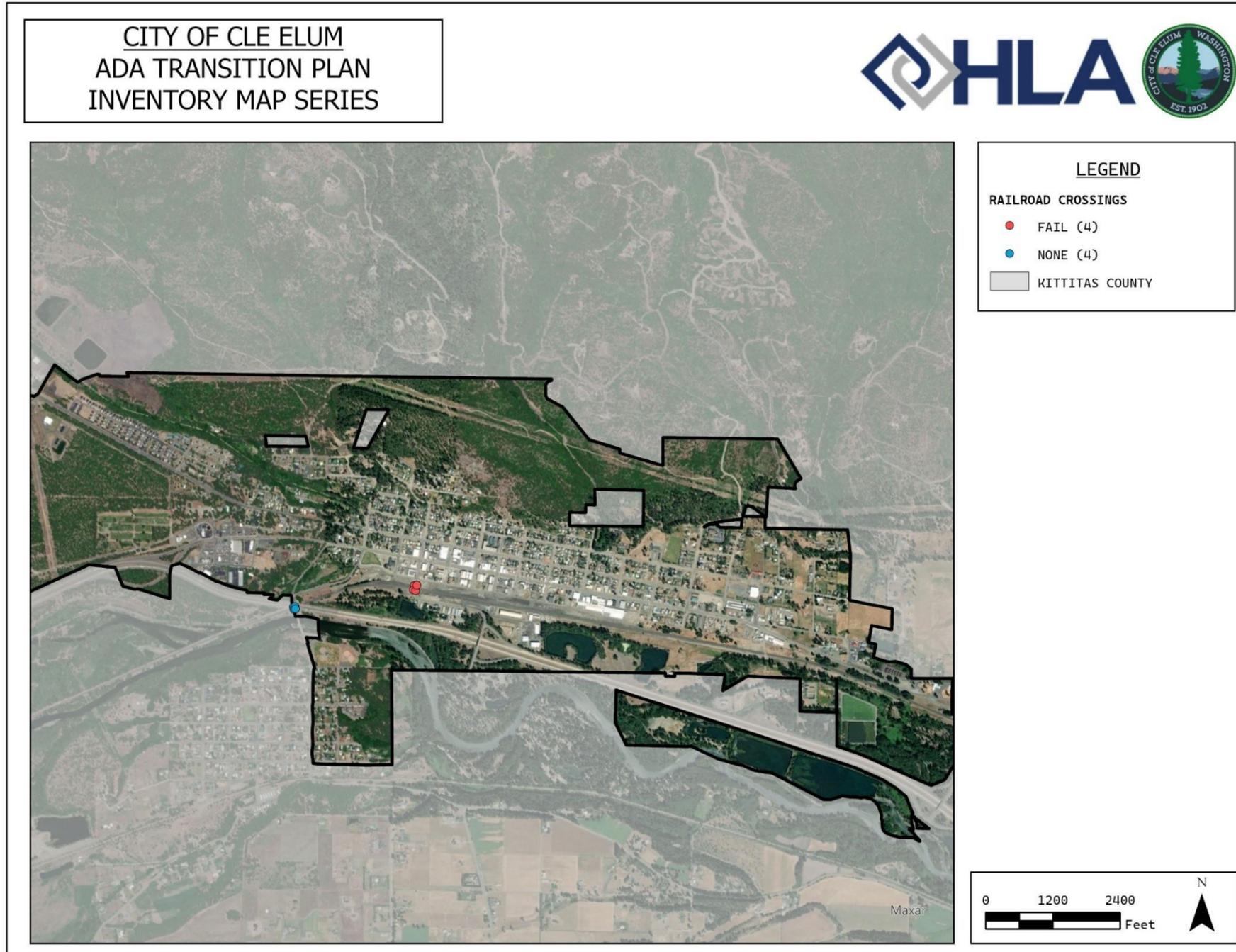
Bike and Multiuse Pathway





ADA SELF-EVALUATION AND
TRANSITION PLAN

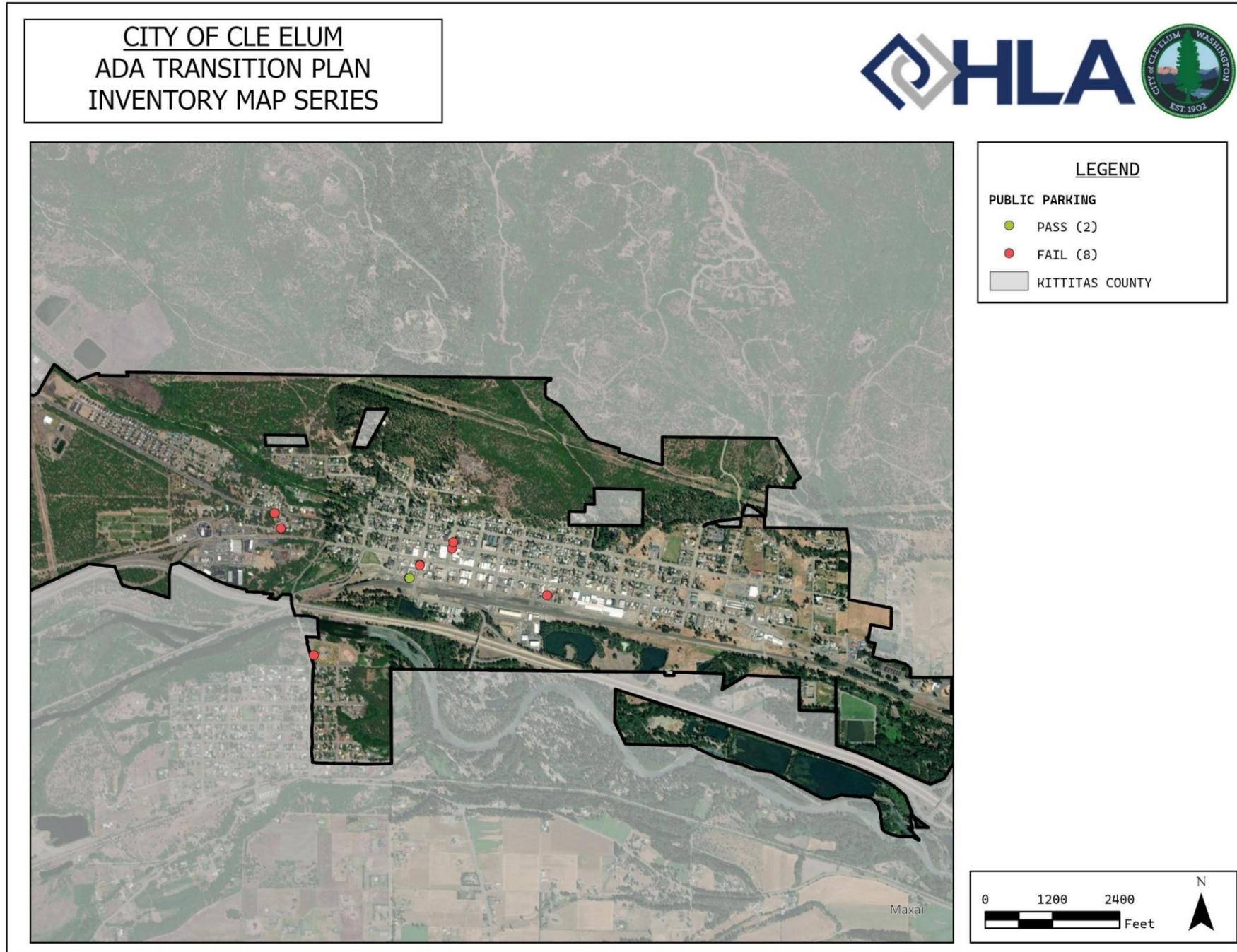
Railroad Crossings





ADA SELF-EVALUATION AND
TRANSITION PLAN

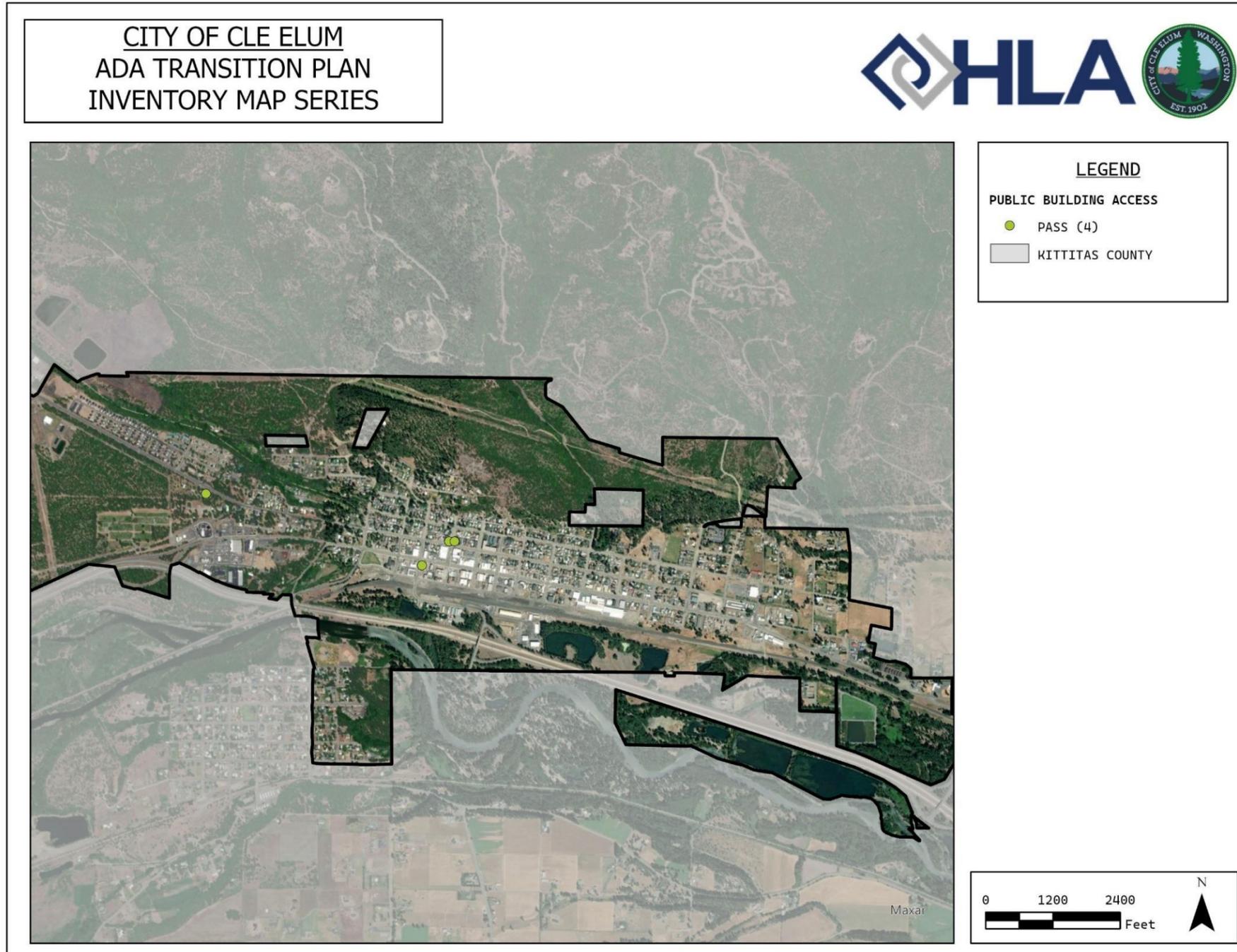
Public Accessible Parking





ADA SELF-EVALUATION AND TRANSITION PLAN

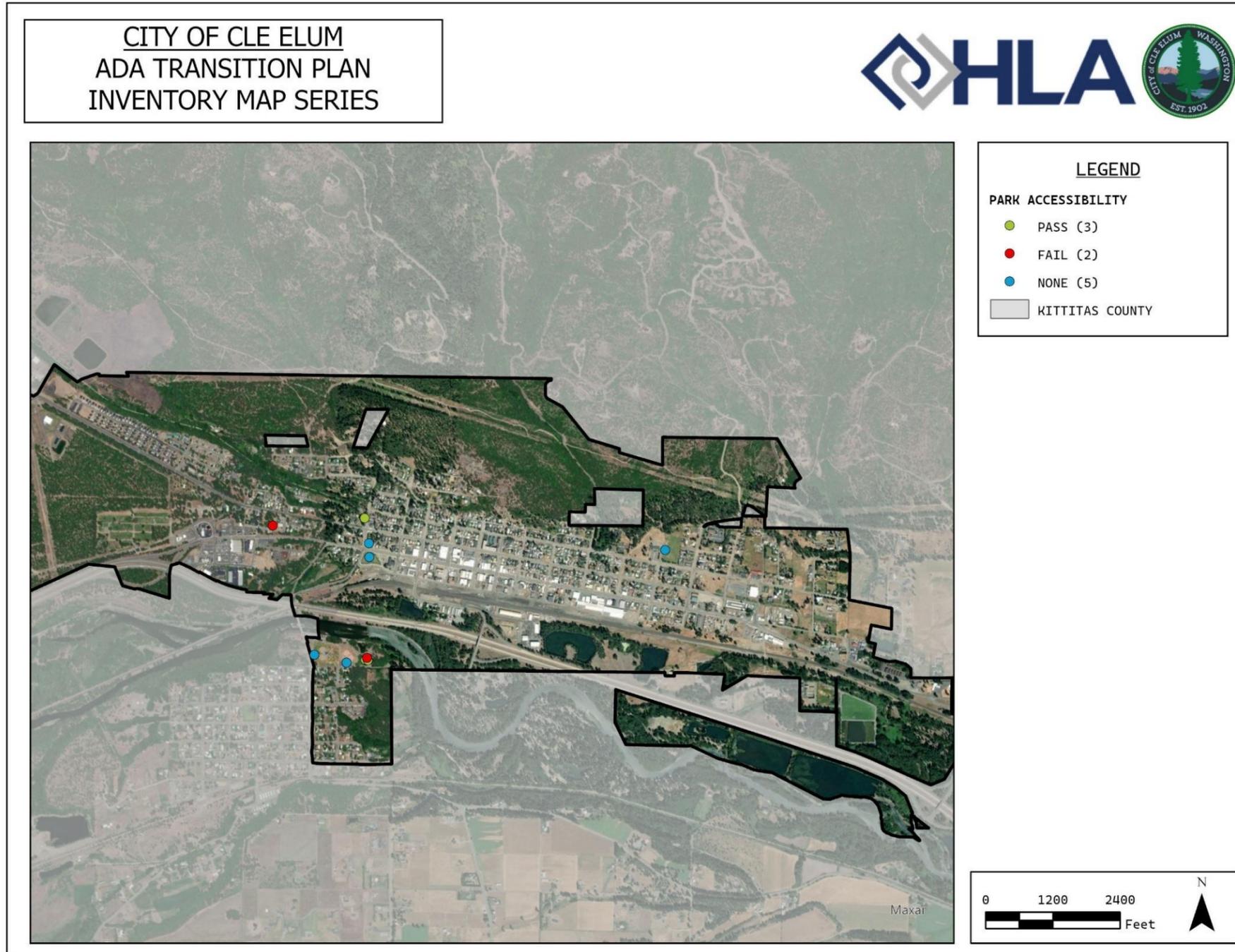
Access to Public Buildings





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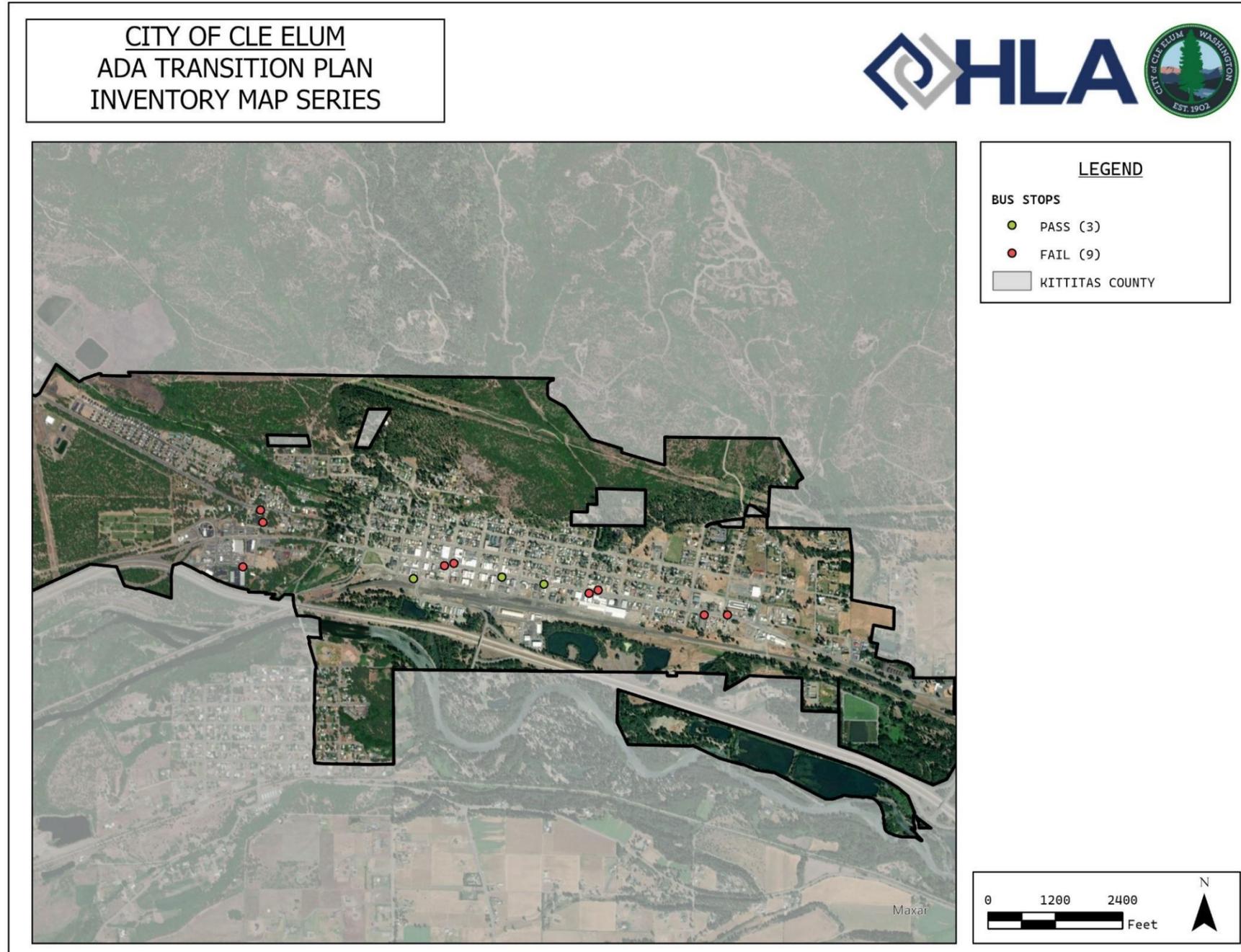
Access to Park Facilities





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Bus Stops





B. PUBLIC COMMENT

INCLUDE COMMENTS SUBMITTED DURING THE PUBLIC COMMENT PERIOD

C. GRIEVANCE PROCEDURE

As part of the ADA requirements the City has posted the following notice outlining its ADA requirements:

PUBLIC NOTICE

In accordance with the requirements of title II of the Americans with Disabilities Act of 1990, Cle Elum will not discriminate against qualified individuals with disabilities on the basis of disability in Cle Elum's services, programs, or activities.

Employment: The city does not discriminate on the basis of disability in its hiring or employment practices and complies with all regulations promulgated by the U.S. Equal Employment Opportunity Commission under title I of the Americans with Disabilities Act (ADA).

Effective Communication: The city will generally, upon request, provide appropriate aids and services leading to effective communication for qualified persons with disabilities so they can participate equally in the Cle Elum's programs, services, and activities, including qualified sign language interpreters, documents in Braille, and other ways of making information and communications accessible to people who have speech, hearing, or vision impairments.

Modifications to Policies and Procedures: The city will make all reasonable modifications to policies and programs to ensure that people with disabilities have an equal opportunity to enjoy all city programs, services, and activities. For example, individuals with service animals are welcomed in city offices, even where pets are generally prohibited.

Anyone who requires an auxiliary aid or service for effective communication, or a modification of policies or procedures to participate in a city program, service, or activity, should contact the office of the City Administrator/ADA Coordinator as soon as possible but no later than 48 hours before the scheduled event.

The ADA does not require the city to take any action that would fundamentally alter the nature of its programs or services, or impose an undue financial or administrative burden.

The city will not place a surcharge on a particular individual with a disability or any group of individuals with disabilities to cover the cost of providing auxiliary aids/services or reasonable modifications of policy, such as retrieving items from locations that are open to the public but are not accessible to persons who use wheelchairs.

City of Cle Elum Grievance Procedure under the Americans with Disabilities Act

This Grievance Procedure is established to meet the requirements of the Americans with Disabilities Act (ADA) of 1990. It may be used by anyone who wishes to file a complaint alleging



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discrimination on the basis of disability in the provision of services, activities, programs, or benefits by the City of Cle Elum.

The complaint should be in writing and contain information about the alleged discrimination such as name, address, and phone number of the complainant, and the location, date, and description of the problem. Alternative means of filing complaints, such as personal interviews or an electronic recording of the complaint, will be made available for persons with disabilities upon request. The complaint should be submitted by the grievant and/or their designee, as soon as possible, but no later than 60 calendar days after the alleged violation to:

Rob Omans, City Administrator
119 West First Street
Cle Elum, WA 98922
Phone: 509-674-2262
TTY/TDD: Dial 711
Email: romans@cleelum.gov

Within 15 calendar days after receipt of the complaint, Rob Omans, or designee, will meet with the complainant to discuss the complaint and the possible resolutions. Within 15 calendar days of the meeting, Rob Omans, or designee, will respond in writing, and where appropriate, in a format accessible to the complainant, such as large print, braille, or electronic means. The response will explain the position of the City of Cle Elum and offer options for substantive resolution of the complaint.

If the response by Rob Omans, or designee, does not satisfactorily resolve the issue, the complainant and/or their designee, may appeal the decision within 15 calendar days after receipt of the response to the Mayor, or their designee.

Within 15 calendar days after receipt of the appeal, the Mayor, or their designee, will meet with the complainant to discuss the complaint and possible resolutions. Within 15 calendar days after the meeting, the Mayor, or their designee, will respond in writing, and, where appropriate, in a format accessible to the complainant, with a final resolution of the complaint.

All written complaints and appeals received by the City of Cle Elum will be retained for at least three (3) years.

Internet

Visit the Cle Elum website and click the "ADA" link to the [ADA Grievance Form](#). Fill in the form online and click "submit." A copy of The ADA Grievance Form is included in this Appendix.

Telephone

Contact the pertinent city staff person listed in the **Contact Information** section of Appendix E to submit an oral grievance. The staff person will utilize the Internet method above to submit the grievance on behalf of the person filing the grievance.

Paper Submittal



Contact the pertinent city staff person listed in the **Contact Information** section of Appendix E to request a paper copy of the city's grievance form, complete the form, and submit it to the ADA Coordinator. A staff person will utilize the Internet method above to submit the grievance on behalf of the person filing the grievance.

The ADA Grievance Form will ask for the following information:

The name, address, telephone number, and email address for the person filing the grievance

The **name, address, telephone number, and email address** for the person alleging an ADA violation (if different than the person filing the grievance)

A description and location of the alleged violation and the nature of a remedy sought, if known by the complainant.

If the complainant has filed the same complaint or grievance with the United States Department of Justice (DOJ), another federal or state civil rights agency, a court, or others, the **name of the agency or court where the complainant filed it and the filing date**.

The city will acknowledge receipt of the grievance to the complainant within 10 working days of its submittal. Cle Elum will also provide to the complainant within 10 working days of its submittal;

A response or resolution to the grievance or;

Information on when the complainant can expect a response or resolution to the grievance. If the grievance filed does not concern a Cle Elum facility, the city will work with the complainant to contact the agency that has jurisdiction.

3. Within 60 calendar days of receipt, a Cle Elum staff person will conduct an investigation necessary to determine the validity of the alleged violation. As a part of the investigation, the staff person would conduct an engineering study to help determine the city's response. The staff person will take advantage of department resources and use engineering judgment, data collected, and any information submitted by the complainant to develop a conclusion. A staff person will be available to meet with the complainant to discuss the matter as a part of the investigation and resolution of the matter. The city will document each resolution of a filed grievance and retain such documentation in the department's ADA Grievance File for a period of seven years.

The city will consider all specific grievances within its particular context or setting. Furthermore, the city will consider many varying circumstances including: 1) the nature of the access to services, programs, or facilities at issue; 2) the specific nature of the disability; 3) the essential eligibility requirements for participation; 4) the health and safety of others; and 5) the degree to which an accommodation would constitute a fundamental alteration to the program, service, or facility, or cause an undue hardship to Cle Elum.



Accordingly, the resolution by Cle Elum of any one grievance does not constitute a precedent upon which the city is bound or upon which other complaining parties may rely.

File Maintenance

The city shall maintain ADA grievance files for a period of seven years.

Complaints of Title II violations may also be filed with the DOJ within 180 days of the date of discrimination. In certain situations, cases may be referred to a mediation program sponsored by the Department of Justice (DOJ). The DOJ may bring a lawsuit where it has investigated a matter and has been unable to resolve violations.

For more information, contact:

U.S. Department of Justice
Civil Rights Division
950 Pennsylvania Avenue, N.W.
Disability Rights Section - NYAV
Washington, D.C. 20530
www.ada.gov

(800) 514-0301 (voice – toll free)

(800) 514-0383 (TTY)

Title II may also be enforced through private lawsuits in Federal court. It is not necessary to file a complaint with the DOJ or any other Federal agency, or to receive a "right-to-sue" letter, before going to court.



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ADA Grievance Form

The City of Cle Elum will make every reasonable effort to ensure that confidentiality is maintained throughout the complaint and investigation process, to the extent consistent with the law, adequate investigation, and appropriate corrective action.

Individual identifying access violation or discrimination

Name:	Address:
Email:	Phone Number:
Authorized Representative:	Address:
Email:	Phone Number:

1. Please describe the City's alleged violation of access requirements, or discriminatory action, in detail. Please include the date, time and location of the incident you are reporting, and the nature of a remedy sought. Add pages if necessary:

2. If the incident involves a City of Cle Elum employee(s) please provide their name(s):

3. If the same complaint or grievance was filed with the United States Department of Justice (DOJ), another federal or state civil rights agency, or others, please provide the name of the agency or court where the complainant filed it and the filing date.

Signature: _____

Date: _____

Signature of (check one)

Observer of alleged access violation Victim of alleged discrimination Authorized Representative

Contact: Mathew Bailey, Public Works Director, 119 W 1st St, Cle Elum, WA 98922 – 509-674-2262, m Bailey@cleelum.gov



D. CONTACT INFORMATION

ADA TITLE II COORDINATOR

Title: Cle Elum City Administrator
Name: Rob Omans
Address: 119 West First Street
Cle Elum, WA 98922
Phone: 509-674-2262
E-mail: romans@cleelum.gov

PUBLIC RIGHT OF WAYS ADA IMPLEMENTATION COORDINATOR

Title: Cle Elum Public Works Director
Name: Mathew Bailey
Address: 119 West First Street
Cle Elum, WA 98922
Phone: 509-674-2262
E-mail: mbailey@cleelum.gov

E. AGENCY ADA DESIGN STANDARDS AND PROCEDURES

DESIGN PROCEDURES

Intersection Corners

Curb ramps or blended transitions will attempt to be constructed or upgraded to achieve compliance within all capital improvement projects. There may be limitations which make it technically infeasible for an intersection corner to achieve full accessibility within the scope of any project. Those limitations will be noted and those intersection corners will remain on the transition plan. As future projects or opportunities arise, those intersection corners shall continue to be incorporated into future work. Regardless of whether full compliance can be achieved or not, each intersection corner shall be made as compliant as possible in accordance with the judgment of city staff.

Sidewalks / Trails

Sidewalks and trails will attempt to be constructed or upgraded to achieve compliance within all capital improvement projects. There may be limitations which make it technically infeasible for segments of sidewalks or trails to achieve full accessibility within the scope of any project. Those limitations will be noted and those segments will remain on the transition plan. As future projects or opportunities arise, those segments shall continue to be incorporated into future work. Regardless of whether full compliance can be achieved or not, every sidewalk or trail shall be made as compliant as possible in accordance with the judgment of city staff.



Traffic Control Signals

Traffic control signals will attempt to be constructed or upgraded to achieve compliance within all capital improvement projects. There may be limitations which make it technically infeasible for individual traffic control signal locations to achieve full accessibility within the scope of any project. Those limitations will be noted and those locations will remain on the transition plan. As future projects or opportunities arise, those locations shall continue to be incorporated into future work. Regardless of whether full compliance can be achieved or not, each traffic signal control location shall be made as compliant as possible in accordance with the judgment of city staff.

Other policies, practices and programs

Policies, practices and programs not identified in this document will follow the applicable ADA standards.

F. GLOSSARY OF TERMS

ADA Transition Plan: WSDOT's transportation system plan that identifies accessibility needs, the process to fully integrate accessibility improvements into the Statewide Transportation Improvement Program (STIP), and ensures all transportation facilities, services, programs, and activities are accessible to all individuals.

Accessible: A facility that provides access to people with disabilities using the design requirements of the ADA.

Accessible Pedestrian Signal (APS): A device that communicates information about the WALK phase in audible and vibrotactile formats.

Alteration: A change to a facility in the public right-of-way that affects or could affect access, circulation, or use. An alteration must not decrease or have the effect of decreasing the accessibility of a facility or an accessible connection to an adjacent building or site.

Americans with Disabilities Act (ADA): The Americans with Disabilities Act; Civil rights legislation passed in 1990 and effective July 1992. The ADA sets design guidelines for accessibility to public facilities, including sidewalks and trails, by individuals with disabilities.

Americans with Disabilities Act Accessibility Guidelines (ADAAG): contains scoping and technical requirements for accessibility to buildings and public facilities by individuals with disabilities under the Americans with Disabilities Act (ADA) of 1990.

Architectural Barriers Act (ABA): Federal law that requires facilities designed, built, altered or leased with Federal funds to be accessible. The Architectural Barriers Act marks one of the first efforts to ensure access to the built environment.

Detectable Warning: A surface feature of truncated domes, built in or applied to the walking surface to indicate an upcoming change from pedestrian to vehicular way.

DOJ: See United States Department of Justice



Federal Highway Administration (FHWA): A branch of the US Department of Transportation that administers the federal-aid Highway Program, providing financial assistance to states to construct and improve highways, urban and rural roads, and bridges.

Pedestrian Access Route (PAR): A continuous and unobstructed walkway within a pedestrian circulation path that provides accessibility.

Pedestrian Circulation Route (PCR): A prepared exterior or interior way of passage provided for pedestrian travel.

PROWAG: An acronym for the *Guidelines for Accessible Public Rights-of-Way* issued in 2005 by the U. S. Access Board. This guidance addresses roadway design practices, slope, and terrain related to pedestrian access to walkways and streets, including crosswalks, curb ramps, street furnishings, pedestrian signals, parking, and other components of public rights-of-way.

Right of Way: A general term denoting land, property, or interest therein, usually in a strip, acquired for the network of streets, sidewalks, and trails creating public pedestrian access within a public entity's jurisdictional limits.

Section 504: The section of the Rehabilitation Act that prohibits discrimination by any program or activity conducted by the federal government.

Transportation Improvement Program (TIP): The TIP includes an annual capital budget and a six-year plan for funding the new transportation construction and reconstruction projects.

Uniform Accessibility Standards (UFAS): Accessibility standards that all federal agencies are required to meet; includes scoping and technical specifications.

United States Access Board: An independent federal agency that develops and maintains design criteria for buildings and other improvements, transit vehicles, telecommunications equipment, and electronic and information technology. It also enforces accessibility standards that cover federally funded facilities.

United States Department of Justice (DOJ): The United States Department of Justice (often referred to as the Justice Department or DOJ), is the United States federal executive department responsible for the enforcement of the law and administration of justice.